



Notice of a public meeting of

Planning Committee

- To:** Councillors Cullwick (Chair), Pavlovic (Vice-Chair), Ayre, Barker, D'Agorne, Daubeney, Doughty, Douglas, Fenton, Fitzpatrick, Hollyer, Kilbane, Warters, Lomas and Fisher
- Date:** Thursday, 13 August 2020
- Time:** 4.30 pm
- Venue:** Remote Meeting

AGENDA

Please note there will be no site visit ahead of this meeting.

1. **Declarations of Interest**

At this point in the meeting, Members are asked to declare:

- any personal interests not included on the Register of Interests
- any prejudicial interests or
- any disclosable pecuniary interests

which they may have in respect of business on this agenda.

2. **Public Participation**

At this point in the meeting, members of the public who have registered their wish to speak can do so.

Please note that our registration deadlines have changed to 2 working days before the meeting, in order to facilitate the management of public participation at remote meetings.

The deadline for registering at this meeting is **5:00pm on Tuesday, 11 August 2020.**

Under current Standing Orders for remote meetings of the Planning Committee, members of the public can speak only on the matters or applications to be considered by the Committee at the meeting.

To register, please contact Democratic Services on the details at the foot of this agenda. You will then be advised on the procedures for dialling into the remote meeting.

Webcasting of Remote Public Meetings

Please note that, subject to available resources, this remote public meeting will be webcast including any registered public speakers who have given their permission. The remote public meeting can be viewed live and on demand at www.york.gov.uk/webcasts.

During coronavirus, we've made some changes to how we're running council meetings. See our coronavirus updates (www.york.gov.uk/COVIDDemocracy) for more information on meetings and decisions.

3. Plans List

This item invites Members to determine the following planning applications:

a) Land South of The Residence, Bishopthorpe Road, York [18/02582/FULM] (Pages 1 - 48)

Erection of 85 apartments in two blocks with seven town houses with associated parking, cycle storage and landscaping (revised scheme) [Micklegate Ward]

b) York City Football Club, Bootham Crescent, York [19/00246/FULM] (Pages 49 - 104)

Erection of 93 dwellings with associated access, infrastructure, landscaping, public open space and parking. [Clifton Ward]

c) 23 Piccadilly, York [19/02563/FULM] (Pages 105 - 150)

Erection of no.132 bed hotel with bar/ restaurant, after demolition of existing office building. [Guildhall Ward]

4. Urgent Business

Any other business which the Chair considers urgent under the Local Government Act 1972.

Democracy Officer

Angela Bielby

Contact details:

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For more information about any of the following please contact the Democratic Services Officer responsible for servicing this meeting:

- Registering to speak
- Business of the meeting
- Any special arrangements
- Copies of reports and
- For receiving reports in other formats

Contact details are set out above.

This information can be provided in your own language.

我們也用您們的語言提供這個信息 (Cantonese)

এই তথ্য আপনার নিজের ভাষায় দেয়া যেতে পারে। (Bengali)

Ta informacja może być dostarczona w twoim własnym języku. (Polish)

Bu bilgiyi kendi dilinizde almanız mümkündür. (Turkish)

یہ معلومات آپ کی اپنی زبان (بولی) میں بھی مہیا کی جاسکتی ہیں۔ (Urdu)

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COMMITTEE REPORT

Date: 13 August 2020 **Ward:** Micklegate
Team: Major and **Parish:** Micklegate Planning
Commercial Team Panel

Reference: 18/02582/FULM
Application at: Land South Of The Residence Bishopthorpe Road York
For: Erection of 85 apartments in two blocks with seven town houses with associated parking, cycle storage and landscaping (revised scheme)
By: Mr D Coppack
Application Type: Major Full Application
Target Date: 17 August 2020
Recommendation: Approve subject to Section 106 Agreement

1.0 PROPOSAL

1.1 Terry's Chocolate Works Bishopthorpe Road comprises the former manufacturing base for the Terry's chocolate and confectionary business. Since closure in 2006 the site has been partially cleared and an Outline Planning Permission ref:- 09/01606/OUTM was granted for the partial re-development of the site for housing. Planning permission is now sought for the erection of 85 apartments in blocks of two to four storeys and 7 three storey town houses on land to the south of the former Multi-Storey Factory (now "The Residence") a Grade II Listed Building. It lies both within the setting of the Listed Building and the Terry's/Racecourse Conservation Area. The proposal has been amended since submission to address concerns in respect of its impact upon the setting of the Listed Building and also to address concerns in terms of landscaping and access layout. It has subsequently been further amended to reduce the number of units and to introduce an element of town houses to the scheme. Further detail has also been submitted in respect of traffic generation modelling and also to update surveys in respect of the ecological value of the site.

2.0 POLICY CONTEXT

2.1 Publication Draft City of York Local Plan Policies (2018)

SS14 Terry' s Extension sites

Application Reference Number: 18/02582/FULM

Item No: 4a

H1 Housing Allocations
H2 Density of Residential Development
H3 Balancing the Housing Market
H10 Affordable Housing
D1 Placemaking
D2 Landscape and Setting
D4 Conservation Areas
D5 Listed Buildings
CC2 Sustainable Design and Construction
ENV2 Managing Environmental Quality
ENV5 Sustainable Drainage
T1 Sustainable Access
G12 Biodiversity and Access to Nature

2.2 York Development Control Local Plan (2005) Policies

CYGP1 Design
CYGP4a) Sustainability
CYGP9 Landscaping
CYHE2 Development in Historic Locations
CYHE3 Conservation Areas
CYHE4 Listed Buildings
CYH2a) Affordable Housing
CYH3c) Mix of Dwellings on Housing Sites
CYH5a) Residential Density
CYED4 Developer Contributions to Educational Facilities
CYL1c) Provision of Open Space on Development Sites
CYNE6) Species protected by law

3.0 CONSULTATIONS

INTERNAL

Public Protection:

3.1 Raise concerns in respect of the proximity of the site to the active area of York Racecourse and recommend a pre-commencement condition in respect of noise insulation should permission be given. Conditions covering remediation of contaminated land together with provision of electrical charge points are also recommended.

Highway Network Management:

3.2 Raise no objections to the scheme as amended on the basis that the modelled traffic generation would not lead to material harm to the safety and convenience of road users on the surrounding highway network. An additional pedestrian/vehicular access through the boundary to Racecourse Road is recommended and has been agreed to by the applicant. That may be subject to condition on any planning permission. In the event of permission being given it is recommended that a sustainable travel plan be conditioned. Section 106 contributions of £15,000 in respect of traffic management measures on Bishopthorpe Road and Campleshon Road, £27,600 to secure implementation of the sustainable travel plan and a contribution of £400 in respect of a bus pass/cycling voucher and £200 in respect of City Car Club Membership for each unit.

Design, Conservation and Sustainable Development (Landscape)

3.3 Raised concerns in relation to the scheme as first submitted relating to the proximity of the western section of the proposed apartments to the surrounding tree cover and the poor layout of the site which gives rise to cumulative harm to the surrounding townscape when combined with the impact of the parking area to the adjacent Multi-Storey Factory. No comment has been made in respect of the revised scheme currently under consideration.

Design, Conservation and Sustainable Development (Design)

3.4 Raise no objection to the proposal as amended on the grounds that the proposed design and mix are acceptable subject to amenity issues being addressed. The impact of the scheme upon the setting of the adjacent Listed Building is now felt to be acceptable taking account of the scale and massing of the previously approved "hybrid" scheme.

Strategic Flood Risk Management

3.5 Were consulted with regard to the proposal. Views will be reported orally at the meeting.

Adult and Housing Services

3.6 Raise no objection to the proposal subject to provision being made within the site to comply with the adopted urban brown field land threshold and secured by means of a Section 106 Agreement. Such provision would comprise 18.4 units with 18 units provided on site and an additional commuted sum.

Children's Services (Education Places)

3.7 Following the withdrawal of pooling restrictions now seek a financial contribution of £446,451 for an estimated additional 23 places required, primary, secondary and early years education contributions in the wider locality.

Community Sport and Recreation

3.8 Raise no objection to the proposal subject to an offsite financial contribution of £8,608 in respect of sports pitches with informal and children's play space being provided by enhancement of the existing Memorial Garden both of which could be secured by Section 106 Agreement.

EXTERNAL:-

Historic England

3.9 No longer object in respect of the revised scheme which it is felt would not lead to material harm to the setting of nearby Listed Buildings.

The North Yorkshire Police Force Designing Out Crime Officer

3.10 Raises no objection to the proposal.

Yorkshire Water Services

3.11 Raise no objection to the proposal subject to any permission being conditioned in respect of surface water drainage.

York CAAP.

3.12 Raise concerns in respect of the poor relationship of the amended scheme to its townscape context and specifically the setting of the adjacent Listed former factory building.

Micklegate Planning Panel

3.13 Object to the proposal on the grounds that even as amended it would be an over-development of the site, it would harm the setting of the adjacent Listed Building and it does not make adequate provision for cycle and vehicle access.

Environment Agency

3.14 Raise no objection to the proposal.

National Trust

3.15 Continue to raise concerns in respect of the level of Heritage related information accompanying the submission and expect a full analysis to be undertaken of such impacts. It is acknowledged that views of the managed site at Goddards are limited by virtue of the surrounding landscaping.

York Civic Trust

3.16 Object to the proposal on the grounds that the proposal represents an over-development of the site, the overwhelming dominance of residential use at the site leads to the creation of an imbalanced community, the development is too tall and would thereby harm the setting of the adjacent former factory building, and it is poor in design terms lacking any clear reference in the wider locality.

4.0 REPRESENTATIONS

4.1 The proposal was publicised by press advertisement and site notice in the locality and a number of neighbouring properties were notified by individual letter. A further publicity exercise was subsequently undertaken in respect of the amended scheme. The proposal has generated a significant degree of public comment and a summary of comments received is outlined below.

4.2 York Racecourse raises concerns in respect of the proximity of residential development to the active racecourse.

4.3 The Terry's of York Planning Action Group(TOYPAG) object to the proposal on the following grounds

- i) the proposal would give rise to substantial harm to the setting of the Terry's/ Racecourse Conservation Area by virtue of the loss of the presently open views across the site;
- ii) the proposal would give rise to substantial harm to the setting of the former Multi-Storey Factory, a Grade II Listed Building without a substantial public benefit to outweigh the harm,
- iii) the proposed apartments would be constructed at too high a density with the consequence that the site would be over-developed;
- iv) the additional levels of traffic generated by the proposal would not be readily accommodated by the designed road layout and would give rise to substantial harm to the safety and convenience of highway users on the surrounding network;
- v) the site can no longer be classified as previously developed land by virtue of the degree to which it has reverted back to nature.

4.4 The Chocolate Works Residents Association object to the proposal on the grounds that

- i) the proposal would give rise to substantial harm to the setting of the Terry's/Racecourse Conservation Area by virtue of the loss of the presently open character of the site;
- ii) the proposal would give rise to substantial harm to the setting of the Multi-Storey Factory, a Grade II Listed Building without any substantial public benefit to outweigh the harm;
- iii) the density of the proposed development would be too high and would represent an over-development of the site;
- iv) the additional levels of traffic generated by the proposal would be too great for the designed road layout and would give rise to substantial harm to the safety and convenience of highway users on the local network.

4.5 Some 169 letters of objection received to the proposal outlining the following planning issues:

- i) the proposal would lead to an oversupply of apartments both within the Terry's development and within the wider area when conventional family housing is required;
- ii) the density of development for the site is too high with the result that the site would be over-developed;
- iii) the proposal would give rise to overflow parking on surrounding side streets and would give rise to conditions prejudicial to the safety and convenience of highway users;

- iv) the proposal would be located too close to the operational racecourse with the result that the amenity of prospective occupants of the site would adversely affected;
- v) comings and goings via the access road to the development would adversely affect users of the Peace Garden public open space and children's play area;
- vi) the development would adversely affect the setting of the Multi-Storey Factory, a Grade II Listed Building;
- vi) the development would adversely affect the setting of the Terry's/Racecourse Conservation Area by virtue of the loss of the presently open character of the site;
- vii) the development represents very poor quality design with very little respect paid to the surrounding context;
- viii) the proposal would harm the residential amenity of apartments within the lower stories of the former Multi-Storey Factory by virtue of a loss of aspect.

4.6 A further publicity exercise was undertaken in respect of the amended which has led to the submission of a further 39 letters raising the following further planning issues:

- i) The adequacy of the ecological surveys in respect of the application site;
- ii) Potential harm to a bat roosting/foraging site;
- iii) The continuing high density of accommodation proposed compared with the quantum of development identified in the Publication Draft Local Plan;
- iv) Concern that the proposal is premature in respect of the progress of the Publication Draft Local Plan pending resolution of the number and mix of new dwellings to be constructed during the duration of the plan period and the final inner boundary of the York Green Belt;
- v) Concern that the application site no longer falls within the NPPF definition of previously developed land by virtue of the extent to which it has returned to its natural character;
- vi) Concern in respect of the lack of mention of the provision of affordable housing within the site;
- vii) Concern that the applicant has failed to examine the impact of the proposal upon the setting of the Listed Building in line with Historic England Guidance on Setting of Heritage Assets;
- viii) Concern that the original approved proposal ref 09/01606/FULM has been disaggregated to avoid complying with the requirements of the original EIA.

5.0 APPRAISAL

KEY CONSIDERATIONS

5.1 KEY CONSIDERATIONS INCLUDE

- Principle of the Development;
- Prematurity;
- Impact upon the setting of the Multi-Storey Factory, a Grade II Listed Building;
- Impact upon the setting of Goddards and its Listed Park and Garden;
- Impact upon the setting of the Terry's/Racecourse Conservation Area;
- Impact upon the Biodiversity and Ecological Value of the Site';
- Design and Layout of the Scheme;
- Impact upon the amenities of prospective occupants of the scheme;
- Impact upon residential amenity of Neighbouring Properties;
- Impact upon the safety and convenience of highway users.

NATIONAL PLANNING POLICY FRAMEWORK

5.2 The revised National Planning Policy Framework was revised in February 2019 (NPPF) and its planning policies are material to the determination of planning applications.

LOCAL PLAN

5.3 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that planning applications be determined in accordance with the development plan unless material considerations indicate otherwise. The Publication Draft City of York Local Plan 2018 ('2018 Draft Plan') was submitted for examination on 25 May 2018. Phase 1 of the hearings into the Local Plan were undertaken in December 2019. In accordance with paragraph 48 of the NPPF as revised in July 2018, the relevant 2018 Draft Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012. (NB: Under transitional arrangements plans submitted for examination before 24 January 2019 will be assessed against the 2012 NPPF).

5.4 The City of York Draft Local Plan Incorporating the Fourth Set of Changes was approved for Development Management purposes in April 2005 (DCLP). Whilst the

DCLP does not form part of the statutory development plan, its policies are considered to be capable of being material considerations in the determination of planning applications where policies relevant to the application are consistent with those in the NPPF as revised in July 2018, although the weight that can be afforded to them is very limited.

POLICY CONTEXT

5.5 IMPACT ON HERITAGE ASSETS: Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 (1990 Act) requires the Local Planning Authority when determining planning applications for development that affects a listed building or its setting to have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses. Section 72 of the 1990 Act requires that with respect to any buildings or other land in a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.

5.6 Case law has made clear that when deciding whether harm to a conservation area or to a listed building or its setting is outweighed by the advantages of a proposed development, the decision-maker must give particular weight to the desirability of avoiding such harm to give effect to its statutory duties under sections 66 and 72 of the 1990 Act. There is a "strong presumption" against the grant of planning permission in such cases.

5.7 NPPF defines listed buildings and conservation areas as 'designated heritage assets'. Paragraph 192 of the NPPF states that with regard to heritage assets when determining applications local planning authorities should take account of:

- a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- c) the desirability of new development making a positive contribution to local character and distinctiveness.

5.8 NPPF paragraph 190 advises that the particular significance of a heritage asset that may be affected by the development proposal should be identified and assessed.

5.9 Paragraph 193 of the NPPF states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the

asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance. Paragraph 194 states that any harm to the significance of a designated heritage asset from development within its setting should require clear and convincing justification.

5.10 The NPPF sets out where a proposed development would lead to substantial harm or to total loss of significance consent should be refused, unless this is necessary to achieve substantial public benefits that outweigh that harm or other specified factors apply (paragraph 195). Paragraph 196 goes on to say that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal.

5.11 The NPPG states that "It is the degree of harm to the asset's significance rather than the scale of the development that is to be assessed. The harm may arise from works to the asset or from development within its setting."

5.12 IMPACT UPON BIODIVERSITY: Central Government planning policy as outlined in paragraph 170 d) of the NPPF indicates that planning decisions should minimise impacts upon and provide net gains for biodiversity including by establishing coherent ecological networks that are more resilient to current and future pressures.

5.13 RESIDENTIAL AMENITY: Central Government Planning Policy as outlined in paragraph 127f) of the NPPF indicates that planning policies and decisions should create places which provide a high standard of amenity for all existing and future users.

PRINCIPLE OF THE DEVELOPMENT

5.14 Section 5 of the NPPF contains the Government's objective of significantly boosting the supply of homes. Section 11 promotes the effective use of land. It states "Planning decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions". It goes on to state that planning decisions should "promote and support the development of under-utilised land and buildings, especially if this would help to meet identified needs for housing where land supply is constrained and available sites could be used more effectively".

5.15 The site currently is underused, previously developed land which is subject to an extant Outline planning permission for a mix of office uses. It is felt that the site fulfils the definition of previously developed land within the glossary to the NPPF by virtue of its previous usage and its continued functional association with the former factory site and also that it has not effectively returned to nature. The city has demonstrable housing need and this is an accessible, sustainable location. Policy is in favour of the proposed re-use of the site in principle.

5.16 The 2018 Draft Plan Policy H2 requires an appropriate density of development on sites to ensure the efficient use of land. Within the York Urban Area developments are expected to achieve a net density of 50 dwellings per hectare. The policy continues that “delivering densities that support the efficient use of land requires good design that responds to its context, an appropriate mix of house types and should be informed by the local character of the area.” Policy H3 states that "proposals for residential development will be required to balance the housing market by including a mix of types of housing which reflects the diverse mix of need across the city. This includes flats and smaller houses for those accessing the housing market for the first time, family housing of 2 to 3 beds and homes with features attractive to older people". The housing mix proposed is 5 x 3 bedroom town houses, 2 x 4 bedroom town houses, 15 x 1 bedroom apartments, 43 x 2 bedroom apartments, and 27 x 3 bedroom apartments. Consequently the proposals do not conflict with the approach proposed in policy H3. The content of Policies H2, H3 and H10 have each been considered at the Local Plan hearings (Phase 1) in the light of the objections and it is felt that moderate weight can be afforded to each in policy terms.

5.17 Affordable housing is proposed in line with Policy H10 of the 2018 Draft Plan and is discussed later in the report.

PREMATURITY

5.18 Objectors have raised concerns that the development may be seen as premature in relation to the delivery of the 2018 Draft Plan in the context of the setting of the inner boundary of the York Green Belt and also the proposed housing number and dwelling mix. Central Government planning policy as outlined in paragraph 49 of the NPPF states that arguments that an application is premature are unlikely to justify a refusal of planning permission other than in the limited circumstances where both:

a) the development proposed is so substantial, or its cumulative effect would be so significant, that to grant permission would undermine the plan-making process by predetermining decisions about the scale, location or phasing of new development that are central to an emerging plan; and

b) the emerging plan is at an advanced stage but is not yet formally part of the development plan for the area.

5.19 Concern has been focussed on two very specific areas, the site is allocated as a strategic allocation giving quantum of 56 dwellings in Policy SS14 of the 2018 Draft Plan (Strategic Allocations) and also the inner boundary of the Green Belt. The proposal envisages the construction of a total of 92 units comprising a mix of apartments and town houses. The figure of 56 units is an estimated yield in the Draft Local Plan based on an indicative average density. As a strategic allocation the site is of significance in delivering a five year housing land supply as required by paragraph 67 of the NPPF and as such is a material consideration in itself. The greater number of units would secure that contribution and would lessen potential pressure in respect of alternative sites. The 2018 Strategic Housing Land Availability Assessment Addendum which forms part of the evidence base of the Local Plan identifies that with the strategic allocations in place there would be a deliverable 5 year Housing Land Supply. However at present there is as little as a 2.77 year supply making reliance on the allocations and their delivery critical. It is felt that the additional numbers are not of such a scale as to be prejudicial to the Local Plan process. Furthermore the plan itself is not at an advanced stage of preparation and as such the tests of prematurity as set out in the NPPF are not met.

5.20 In terms of the boundary of the general extent of the Green Belt as defined in saved RSS policies YH9(c) and Y1(C1 and C2) , the boundary lies beyond the site to the south west. Historically the site has been developed from an early stage to house a variety of ancillary structures associated with the chocolate manufacturing process. It has therefore a high degree of functional association with the former multi-storey factory and not with the more low intensity open uses associated with the racecourse and with Bustardthorpe allotments beyond. It was in that context that permission was given in the part implemented Outline permission 09/01606/OUTM for a mixed use scheme on the site. To consider the five purposes of Green Belt designation as outlined in paragraph 134 of the NPPF, specifically to check the unrestricted sprawl of urban areas; to prevent encroachment into open countryside and to safeguard the setting of Historic Cities, it is felt that the application site is clearly urban land albeit located at the urban fringe. It should not therefore be treated as falling within the general extent of the Green Belt.

IMPACT UPON THE SETTING OF THE MULTI-STOREY FACTORY

5.21 ASSET SIGNIFICANCE- The former Multi-Storey Factory which is the closest of the retained structures from the former chocolate works to the development is a Grade II Listed Building. The former Time Office which is also in close proximity to the site is similarly Grade II Listed. The wider complex is designed to a simple Neo Classical metaphor characteristic of the early 1920s with each building constructed in brick with art-stone detailing. The remainder of the complex lies to the rear aligned on the former principal access, now The Boulevard, with prominent Art Deco detailing in art stone on the Clock Tower and Liquor Store to the rear. The complex is a strong group in architectural terms presenting a clear strong architectural style reflecting the position and strength of branding of the Terry's Chocolate firm at the point where the complex was developed. The complex also has strong historic interest representing the best surviving evidence of York's pivotal role in the development of the industry with the Multi-Storey Factory and the Clock Tower making their own particularly iconic contribution to the City skyline.

5.22 ASSESSMENT OF IMPACT- The setting of the building is characterised by the iconic view of the Multi-Storey Factory on the approach to the City from Bishop Thorpe to the south west and from the environs of the Racecourse nearby. The view is characterised by a high degree of visual permeability particularly during the winter months when the surrounding trees and other landscaping are not in leaf. The scheme as initially submitted envisaged the erection of two four storey apartment blocks in an inverted U plan providing some 123 units. That would however, have effectively taken away the visual permeability of the site and thereby eroded the setting of the Multi-Storey Factory. Subsequently the scheme has been amended to significantly reduce its scale. The fourth storey on each block has been recessed. Block 2 has been significantly reduced in size and made linear running parallel with Bishopthorpe Road. At the same time a block of 7 town houses of 3 stories are proposed running parallel with the boundary of the adjacent Peace Garden. The central section of Block 1 has also been lowered to two storeys. The amendments result in the visual permeability of the site being retained and taking account of the previous office based scheme approved within 09/01606/OUTM, it is felt that the setting of the former factory would be broadly safeguarded. Taking account of paragraph 196 of the NPPF it is felt that the level of harm to the setting of the former factory from the scheme as amended would be within the range of "less than substantial". The harm would be clearly outweighed by the public benefit of the development of a sustainably located previously developed site for residential use in line with the requirements of the NPPF with associated additional construction employment.

IMPACT UPON THE SETTING OF GODDARD'S AND ITS HISTORIC PARK AND GARDEN

5.23 Concerns have been raised by objectors in respect of the impact of the proposals on the setting of Goddards the Grade I Listed former home of the Terry family with its historic park and garden on the north side of the Racecourse. Within the terms of the NPPF these are designated Heritage Assets. It is suggested that the original factory complex was constructed to be clearly intervisible with the family residence and as such forms part and parcel of its wider setting.

5.24 ASSET SIGNIFICANCE: Goddard's comprises a Grade I Listed Victorian brick built house set within large grounds lying behind the Tadcaster Road frontage on the western approach to the City Centre. It was the principal residence of the Terry family, operators of the chocolate works and its gardens, which are also Listed as historic park and garden at Grade II, were modelled to create views across the area of the racecourse of the Chocolate Works beyond. As such both complexes of buildings form part of a wider functional setting.

5.25 ASSESSMENT OF IMPACT: Goddard's House lies in significant landscaped grounds accessible from Tadcaster Road to the north west of the site beyond the Racecourse. The principal views of the former Chocolate Works from Goddards are of the western and north western elevations of the former Multi-Storey Factory set across the background of the Racecourse. Whilst it is clearly acknowledged that there has been some significant harm to the visual inter-relationship of Goddards with the former chocolate works by virtue of developments at the Racecourse it is felt that the alignment of the proposed development together with the mature landscaping at the site boundary mean that any harm arising from the new proposal would effectively be minimal. Within the terms of paragraph 196 of the NPPF it is felt that the level of harm arising to Goddards and its associated historic park and garden is less than substantial at the lower end of the scale. It is felt that the public benefit brought about by the development of a sustainably located previously developed site for residential use in line with the requirements of the NPPF with associated additional construction employment would more than outweigh any harm.

IMPACT UPON THE SETTING OF THE TERRY'S/RACECOURSE CONSERVATION AREA

5.26 ASSET SIGNIFICANCE: The Terry's/Racecourse Conservation Area comprises the retained buildings of the Chocolate Works complex together with the Racecourse complex. It is characterised by agglomerations of comparatively high buildings some of which such as the Racecourse main stand, the former Multi-Storey Factory building and the Clock Tower have iconic status within the wider City skyline. The former Terry's character area includes all the retained buildings of the former complex. It is of special interest by virtue of the high quality design approach with the factory layout aligned in a grid with two points of access, with the generally inward facing nature of the site. The key spaces are the main boulevard, the forecourt to the Headquarters Building and the garden to the south west.

5.27 ASSESSMENT OF IMPACT: The proposal as originally submitted would have resulted in a physically divorced but visually poorly related development that would have competed visually with the well-ordered iconic layout of the Conservation Area. The scheme as amended would be more subservient in its scale and massing and by introducing a more linear and visually permeable element in closest proximity to Bishopthorpe Road safeguard the relationship of the site to the wider complex. The setting of the Conservation Area is thereby secured. It is felt in the context of paragraph 196 of the NPPF that the level of harm to the setting of the Conservation Area would be less than substantial and the harm would be outweighed by the public benefit of the development of a sustainably located previously developed site for residential use in line with the requirements of the NPPF with associated additional construction employment.

CONCLUSION ON HERITAGE ASSETS

5.28 It is felt that there would be a demonstrable impact from the proposal upon the setting of designated Heritage Assets within the immediate area. Impact upon the setting of the Listed former Multi-Storey Factory is probably the more notable with the development proposal sat within the iconic view of its south western elevation. It is however felt that the amended scheme would lead to a level of harm that would be less than substantial in terms of paragraph 196 of the NPPF and that the harm would be clearly outweighed by the public benefit of the development of a sustainably located previously developed site for residential use in line with the requirements of the NPPF and the Draft Local Plan with associated additional construction employment. The harm arising from the impact upon the setting of the Terry's/ Racecourse Conservation along with Goddard's House and its park and garden is similarly felt to be less than substantial with the public benefit of the development of a sustainably located previously developed site for residential use in

line with the requirements of the NPPF and the Draft Local Plan with associated additional construction employment, outweighing any harm.

IMPACT UPON THE ECOLOGICAL AND BIODIVERSITY VALUE OF THE SITE

5.29 Policy G12 of the 2018 Draft Plan indicates that new development should result in a net gain to and help improve biodiversity. In view of the low level of objection to the Policy and the position following on from the first phase of hearings it is felt that the Policy carries moderate weight. The site comprises an area of previously developed land surrounded by a belt of mature trees on three sides which define its character within the wider townscape. The proposal has been accompanied by a phase 1 Ecological Report in which the possible presence of a series of protected species is examined. Features potentially of use to bats have been identified within the tree belt surrounding the site on three sides although no direct evidence of bat roosting has been found within the area of the application site. Anecdotal evidence has been forthcoming in respect of bat foraging taking place within the wider area particularly within the adjacent racecourse site and also the Peace Garden. Subsequent to submission of the application further surveys have been undertaken which confirm that there is no evidence of bat roosting within the boundaries of the site. The proposed development would not give rise to direct harm to that activity although external mounted lighting of any description would have a clearly disruptive impact. It is therefore recommended that any planning permission be conditioned to require the formal approval of any lighting scheme and also to provide suitable bat nesting facilities within the new buildings.

5.30 In terms of other species significant to local biodiversity ground conditions are not suitable to provide habitat for Great Crested Newt or other species of amphibian or reptile. Badgers and hedgehogs are recorded anecdotally as being present within the wider area but surveys undertaken at the site have not shown any evidence of their direct presence nearby. In terms of bird species there is no evidence of ground nesting birds within the site and no work is proposed to the existing tree cover within the site boundaries. The submitted arboricultural method statement furthermore sets out measures to protect nesting birds whilst construction work takes place.

5.31 Subject to any permission being conditioned in terms of external lighting and also to provide bat nesting facilities within the new building the development is felt to be acceptable in terms of biodiversity and the requirements of Policy G12 are complied with.

DESIGN AND LAYOUT OF THE SCHEME:

5.32 Central Government Planning Policy as outlined in paragraph 127 b), c) and d) of the NPPF indicates that planning decisions should create places which are visually attractive as a result of good architecture, layout and appropriate and effective landscaping, are sympathetic to local character and history including the surrounding built environment and landscape setting and establish a strong sense of place using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live. Paragraph 130 also indicates that obviously poor designs that fail to take account of opportunities for improving the character and quality of the area should be refused.

5.33 Policy HW7 of the 2018 Draft Plan indicates that new residential developments should demonstrate that they incorporate well designed streetscapes which encourage people to spend time outdoors, provide good, easy to navigate pathways to encourage physical activity and good connections to neighbouring communities and green spaces. Draft Policy D1 indicates that development proposals should demonstrate that the proposed density would be appropriate to the proposed use and its surrounding context.

5.34 The scheme as originally submitted envisaged the construction of two substantial four storey blocks of apartments on a built footprint which would have covered almost the entire width of the site with much of the remainder of the site taken up with car parking which would have been co-terminous with the parking area for the former Multi-Storey Factory directly to the north. The result was a physically inward looking site which paid scant reference to its surroundings and gave an appearance of clear “over development” all set within a sea of parking. The scheme has subsequently been progressively amended. The overall parking area has been reduced in size and broken up with the incorporation of individual trees and landscaping beds.

5.35 In terms of the design of the scheme, overall numbers have been reduced by 31 units and the layout in itself has been re-designed in part to safeguard the setting of the Listed factory building and the Terry's/Racecourse Conservation Area. The effect of the amendments has been to add a greater degree of variety in terms of forms and materials at the same time as reducing the overall quantum of development. The U shaped Block 1 now has a much lower central section with the upper storeys of the wings set back. Block 2 is now linear in form and paired with a linear block of 7 townhouses. The overall result is to create a development that

much more clearly respects its context and secures compliance with Policy D1 of the 2018 Draft Plan as well as paragraph 127 of the NPPF.

LIVING CONDITIONS OF FUTURE RESIDENTS

5.36 Central Government Planning Policy (as outlined in paragraph 127f) of the NPPF indicates that planning policies and decisions should create places which give rise to a high standard of amenity for all existing and future users. The proposal envisages the erection of 85 residential apartments and 7 town houses located close to the boundary with York Racecourse directly to the north west. In addition to conventional race meetings the race course hosts music concerts and other "noisy" events throughout the year into late evening. Those apartments within the western block would be highly vulnerable to noise and related disturbance from activities taking place directly adjacent. The submitted application details indicate an approximate distance of 80 metres between the north west of the apartments and the edge of the active race course. Complaints in respect of noise nuisance from events at the race course have been frequent in respect of earlier phases of the development scheme. The impact upon the occupants of the current proposal would be more severe by virtue of the closer proximity to the active areas of the race course and the lack of shelter afforded by high rise elements of the race course development.

5.37 The degree of harm to amenity would be mitigated by a scheme of internal noise insulation which can be conditioned as part of any permission. Further concerns have previously been raised by consultees in respect of the amenity relationships between several of the units. These have now been resolved in respect of the most recent amendments. As a consequence impact upon the amenities of prospective residents of the development would not be sufficient as to amount to a reason for refusal in itself.

IMPACT UPON THE RESIDENTIAL AMENITY OF NEIGHBOURING PROPERTIES

5.38 Concern has been expressed by residents of apartments within the lower floors of the Multi-Storey Factory in respect of a loss of aspect arising from the proposed development. The new development would be approximately 73 metres from the Multi-Storey Factory at its closest point. The significant distance between the two buildings would not result in any issue of over-looking, over-shadowing or loss of privacy. In terms of a loss of aspect the application site is presently characterised by its openness with longer distance oblique views of the area of the racecourse to the

south. The significant degree of amendment to the scheme which has re-introduced a degree of visual permeability as well as greater degree of variation in the overall layout, as a result it would not now give rise to a material loss of aspect.

IMPACT UPON THE SAFETY AND CONVENIENCE OF HIGHWAY USERS

5.39 Policy T1 of the Draft Local Plan indicates that development will be supported where safe and appropriate access is provided to the adjoining highway, sufficient secure cycle parking is provided for user's needs and safe and secure layouts are provided for motorised vehicles. Concerns have been expressed by objectors in respect of the level of vehicle parking proposed within the site and the potential for a significant issue of overflow parking in surrounding side streets together with the capacity of the vehicular access to the site to cater for the anticipated traffic flows. The applicant has submitted a revised Transport Statement which identifies trip generation as significantly below that modelled in respect of the office development approved for the site in respect of 09/01606/FULM. The provision of secure cycle parking in respect of the proposed town houses is at the same time confirmed at the rate indicated in the Authority's parking standards.

5.40 120 car parking spaces and 132 cycle parking spaces would be provided. The car parking spaces provide a rate of 1.25 spaces per unit supported by sustainable transport measures and parking controls in surrounding streets. The rate of provision parallels that previously approved in respect of the David Wilson Homes development to the north. A vehicular access to the site was previously constructed in combination with the residential conversion of the former Multi-Storey Factory on the basis of the application site being developed for a large hotel. In addition to a link for vehicular traffic this provides access for cyclists and pedestrians to Bishopthorpe Road, the Peace Garden, the nearby Sustrans cycle route and the remainder of the wider development. A further pedestrian cycle link from the site onto Racecourse Road to the south west has been suggested to improve connectivity. This has been accepted by the applicant and can be secured by a Grampian style condition as part of any planning permission. The requirements of Policy T1 are now felt to be complied with in respect of the amended scheme.

DRAINAGE AND FLOOD RISK

5.41 Paragraph 163 of the NPPF indicates that when determining planning applications local planning authorities should ensure that flood risk is not increased elsewhere. The application site is in Flood Zone 1 and therefore at the lowest

assessed risk of river flooding. The wider site has been subject to a comprehensive surface water drainage scheme which the development would connect into and which has capacity to receive the associated flows the development is therefore felt to be acceptable in drainage/flood risk terms subject to the comments of the Senior Flood Risk Engineer.

LANDSCAPE/PUBLIC REALM

5.42 Policy D1 of the Draft Local Plan indicates that development proposals should enhance the existing pattern of landscape, planting and boundary treatment. The application site is located within clearly defined landscaped boundaries that would be protected during the course of development. The principal apartment block is centred around a courtyard amenity area facing southwards to take advantage of its relationship with the boundary landscape treatment. The proposed hard landscaping materials would reflect those previously adopted for the earlier elements of the wider development to the north. The amended scheme is therefore felt to be acceptable in terms of the requirements of Policy D1.

OTHER ISSUES

5.43 Concern has been expressed by an objector in respect of references made to the Outline Planning Permission ref 09/01606/OUTM on the basis that it has never been implemented and is not therefore a material consideration. It is also contended that the Outline Planning Permission was subject to Environmental Impact Assessment under Schedule 2 of the Regulations and that the site has subsequently been developed piecemeal in order to avoid the need for a further EIA. The Outline Planning Permission has however been partially implemented in respect of the northern part of the site. The site has been developed in sections with individual stand alone schemes as a consequence solely of subsequent land ownership issues. The larger schemes have been re-screened where within the relevant threshold of the current EIA Regulations.

SECTION 106 ISSUES

5.44 Policy DM1 of the 2018 Draft Plan indicates that the Local Planning Authority will seek financial contributions from developers to ensure that the necessary infrastructure is in place in order to support the development. Development will not be permitted unless that infrastructure is available and able to cater for the local and wider strategic demand generated by the proposal.

5.45 SUSTAINABLE TRANSPORT- Financial contributions of £400 per dwelling are sought in respect of an annual cycle contribution/bus pass on first occupation of each unit and £200 per dwelling in respect of City Car Club membership. In addition contributions of £15,000 towards enhancement of parking management on Bishopthorpe Road and Campleshon Road are sought together with £27,600 to secure the implementation of the sustainable travel plan.

5.46 EDUCATION- In terms of Educational provision contributions are sought of a total of £446,451. Broken down this comprises some £182,370 in respect of early years provision, £99,948 towards the Millthorpe School Phase 2 improvement project and £164,133 to reconfigure small group teaching/work rooms at Knavesmire School.

5.47 OPEN SPACE- A contribution of £8,608 towards improvements to playing pitches within 2 kilometres of the site is sought with improvements to the existing children's play space within the adjacent Peace Garden which is in the control of the applicant. The detail of the improvements would be resolved through a Section 106 Agreement.

5.48 AFFORDABLE HOUSING- The proposal falls to be considered within the remit of Policy H10 of the Publication Draft Local Plan. The policy has a target of 20% affordable housing on brownfield sites within the urban area. 18 units would be provided on site with an off-site financial contribution in respect of 0.4 of a unit. The detailed tenure mix would be resolved through a Section 106 Agreement.

5.49 The proposed obligations are considered to comply with Regulation 122 of the 2010 CIL Regulations in that the obligations are;

- a) necessary to make the development acceptable in planning terms,
- b) directly related to the development,
- c) fairly and reasonably related in scale and kind to the proposed development.

6.0 CONCLUSION

6.1 The application proposes a development of 92 residential units (85 apartments and 7 townhouses) on previously developed land in a sustainable location. The development would make an appreciable contribution to the City's five year housing land supply as required by paragraph 67 of the NPPF. Outline planning permission has previously been given for the office element of a mixed use scheme on the site with a similar scale and massing. It is felt that the scheme as amended would give

rise to less than substantial harm to the setting of the Multi-Storey Factory, Goddard's House and its park and garden or the Terry's Racecourse Conservation Area. The harm caused is felt to be outweighed by the public benefit of the development of a previously developed site in a sustainable location for residential use with in line with the requirements of the NPPF with associated additional employment in construction .The scale and design of the proposal would not harm the living conditions of nearby residents and subject to any permission being appropriately conditioned the amenity of existing residents would be safeguarded. Parking would be provided in accordance with the standards outlined in the DCLP and traffic generation would be lower than that previously identified in respect of the approved Outline scheme.

6. 2 In the light of these conclusions, there are no protective policies within the NPPF which provide a clear reason for refusal. The application of the presumption in favour of sustainable development in paragraph 11 of the NPPF therefore means that planning permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

7.0 RECOMMENDATION:

i That delegated authority be given to the Assistant Director for Planning and Public Protection to APPROVE the application subject to

a. the completion of a Section 106 Agreement to secure the following obligations

- Sustainable travel - £200/dwelling contribution towards cycle provision and £200/dwelling towards car club (£400 in total per dwelling)(Index linked) £15000 towards enhancement of parking management on Bishopthorpe Road and Campleshon Road
- Off-site play provision - £8608(index linked) towards improvement of playing pitches within 2 kilometres of the site. Improvements to be sought to the children's play space within the adjacent Peace Garden, details to be agreed
- Education- £182, 370 in respect of early years provision, £99,948 towards Millthorpe School Phase 2 improvement project and £164,133 to reconfigure small group teaching rooms at Knavesmire School.
- Affordable Housing - onsite provision of 18 dwellings and an offsite contribution in lieu of 0.4 equivalent of a dwelling unit.

b. the conditions set out below.

ii The Assistant Director be given delegated authority to finalise the terms and details of the s106 Agreement

iii The Assistant Director be given delegated powers to determine the final detail of the planning conditions.

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans:-

Drawing Refs:7988 (500) 100 K ;7988 P (000) 050 C ;7988 P(000)000 N ;7988 P(000)001 A ; 7988 P(000)010 G ; 7988 P(000)011 I ; 7988 P(000)012 I ; 7988 P(000)013 I ; 7988 P(000)014 F ; 7988 P(000)015 H ; 7988 P(000)016 G ; 7988 P(000)017 H ; 7988 P(000)020 H ; 7988 P(000)030 E ; 7988 P(000)100 I ;7988 P(000)101 I ; 7988 P(000)102 B ;7988 P(000)200 B ; 7988 P(000)201 ; 7988P(000)030 E ; 7988 P(000) 000 M ; 7988 P(000) 001 B ; 7988 P(000) 020 I ;7988 P(000) 050 E ; 7988 P(000) 100 J ; 7988 P(000) 101 J ; 7988 P(000) 102 C ; 7988 P(000) 110 E ; 7988 P(000) 111 D

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 Notwithstanding any proposed materials specified on the approved drawings or in the application form submitted with the application, samples of the external materials to be used shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the construction of the development beyond foundation level. The development shall be carried out using the approved materials.

Note: Because of limited storage space at our offices it would be appreciated if sample materials could be made available for inspection at the site. Please make it clear in your approval of details application when the materials will be available for inspection and where they are located.

Reason: So as to achieve a visually cohesive appearance.

4 A sample panel of the brickwork to be used on this building shall be erected on the site and shall illustrate the colour, texture and bonding of brickwork and the mortar treatment to be used, and shall be approved in writing by the Local Planning Authority prior to the commencement of building works beyond foundation level. This panel shall be retained until a minimum of 2 square metres of wall of the approved development has been completed in accordance with the approved sample.

Reason: So that the Local Planning Authority may be satisfied with the finished appearance of these details prior to the commencement of building works in view of

their sensitive location.

5 Details of all means of enclosure to the site boundaries shall be submitted to and approved in writing by the Local Planning Authority before the construction of the development commences beyond foundation level and shall be provided in accordance with the approved details before the development is occupied.

Reason: In the interests of the visual amenities of the area and the amenities of neighbouring properties.

6 The development shall not be occupied until there has been submitted and approved in writing by the Local Planning Authority a detailed landscaping scheme which shall illustrate the number, species, height and position of trees and shrubs. This scheme shall be implemented within a period of six months of the completion of the development. Any trees or plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of a similar size and species, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the site in the interests of the character and appearance of the area.

7 The development hereby authorised shall be undertaken in strict accordance with the requirements of Arboricultural Method Statement Ref: AWA2427 AMS. Dated November 2018

Reason: To ensure protection of existing trees before, during and after development which are covered by a Tree Preservation Order and/or make a significant contribution to the amenity of the area.

8 Details of the reduction in carbon emissions the development hereby approved would achieve when compared against Part L of the Building Regulations (the notional building) shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the construction of the buildings and the development shall be carried out in accordance with the approved details.

The details shall demonstrate a reduction in carbon emissions of at least 28% through the provision of renewable or low carbon technologies or through energy efficiency measures with at least a 19% reduction in dwelling emission rate compared to the Target Emission Rate (calculated using Standard Assessment Procedure methodology as per Part L1A of the Building Regulations).

Details shall also be submitted that demonstrate that the development shall also achieve a water consumption rate of no more than 110 litres per person per day (calculated as per Part G of the Building Regulations).

Reason: In the interests of sustainable design and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

9 No development shall take place until a detailed scheme of noise insulation measures for protecting the approved residential buildings from externally generated noise has been submitted to and approved in writing by the Local Planning Authority. Upon completion of the insulation scheme works no part of the development shall be occupied until a noise report demonstrating compliance with the approved noise insulation scheme has been submitted to and approved in writing by the Local Planning Authority.

Reason: To protect the amenity of people living in the new property from externally generated noise and in accordance with the National Planning Policy Framework.

INFORMATIVE TO CONDITION 9: The building envelope of all residential accommodation shall be constructed so as to achieve internal noise levels in habitable rooms of no greater than 35 dB LAeq (16 hour) during the day (07:00-23:00 hrs) and 30 dB LAeq (8 hour) and LAFMax level during the night (23:00-07:00 hours) should not exceed 45dB(A) on more than 10 occasions in any night time period in bedrooms and should not regularly exceed 55dB(A). These noise levels shall be observed with all windows open in the habitable rooms or if necessary windows closed and other means of ventilation provided.

10 Prior to commencement of the development, a Construction Environmental Management Plan (CEMP) for minimising the creation of noise, vibration and dust during the demolition, site preparation and construction phases of the development shall be submitted to and approved in writing by the Local Planning Authority. The CEMP must include a site specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and include a package of mitigation measures commensurate with the risk identified in the assessment. All works on site shall be undertaken in accordance with the approved scheme, unless otherwise approved in writing by the Local Planning Authority.

Reason: In the interests of the living conditions of nearby residential property.

INFORMATIVE TO CONDITION 10

For noise details on hours of construction, deliveries, types of machinery to be used, use of quieter/silenced machinery, use of acoustic barriers, prefabrication off site etc, should be detailed within the CEMP. Where particularly noisy activities are expected to take place then details should be provided on how they intend to lessen the impact i.e. by limiting especially noisy events to no more than 2 hours in

duration. Details of any monitoring may also be required, in certain situation, including the location of positions, recording of results and identification of mitigation measures required.

For vibration details should be provided on any activities which may results in excessive vibration, e.g. piling, and details of monitoring to be carried out. Locations

of monitoring positions should also be provided along with details of standards used for determining the acceptability of any vibration undertaken. In the event that excess vibration occurs then details should be provided on how the developer will deal with this, i.e. substitution of driven pile foundations with auger pile foundations. Ideally all monitoring results should be recorded and include what was found and mitigation measures employed (if any).

With respect to dust mitigation, measures may include, but would not be restricted to, on site wheel washing, restrictions on use of unmade roads, agreement on the routes to be used by construction traffic, restriction of stockpile size (also covering or spraying them to reduce possible dust), targeting sweeping of roads, minimisation of evaporative emissions and prompt clean up of liquid spills, prohibition of intentional on-site fires and avoidance of accidental ones, control of construction equipment emissions and proactive monitoring of dust. Further information on suitable measures can be found in the dust guidance note produced by the Institute of Air Quality Management, see <http://iaqm.co.uk/guidance/>. The CEMP must include a site specific risk assessment of dust impacts in line with the IAQM guidance note and include mitigation commensurate with the scale of the risks identified.

For lighting details should be provided on artificial lighting to be provided on site, along with details of measures which will be used to minimise impact, such as restrictions in hours of operation, location and angling of lighting.

In addition to the above the CEMP should provide a complaints procedure, so that in the event of any complaint from a member of the public about noise, dust, vibration or lighting the site manager has a clear understanding of how to respond to complaints received. The procedure should detail how a contact number will be advertised to the public, what will happen once a complaint had been received (i.e. investigation), any monitoring to be carried out, how they intend to update the complainant, and what will happen in the event that the complaint is not resolved. Written records of any complaints received and actions taken should be kept and details forwarded to the Local Authority every month during construction works by email to the following addresses public.protection@york.gov.uk and planning.enforcement@york.gov.uk

11 Provision of Electric Vehicle Charging Points

12 Prior to development, a detailed remediation scheme to bring the site to a

condition suitable for the intended use (by removing unacceptable risks to human health, buildings and other property and the natural and historical environment) shall be submitted to and approved in writing of the Local Planning Authority. The scheme shall include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme shall ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

13 Prior to first occupation or use, the approved remediation scheme shall be carried out in accordance with its terms and a verification report that demonstrates the effectiveness of the remediation carried out must be produced and is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems.

14 In the event that contamination is found at any time when carrying out the approved development that was not previously identified, it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

15 The development hereby approved shall not be occupied until a Full Travel Plan for the college has been submitted and approved in writing by the LPA. The travel plan should be developed and implemented in line with local and national guidelines. The site shall thereafter be occupied in accordance with the aims, measures and outcomes of said Travel Plan.

Within 12 months of first occupation of the development approved a first year travel survey shall have been submitted to and approved in writing by the LPA. Results of yearly travel surveys shall then be submitted annually to the authority's travel plan officer for approval.

Reason: To ensure the development complies with local and national transportation and planning policies to ensure adequate provision is made for the movement of vehicles, pedestrians, cycles and other forms of transport to and from the site, together with parking on site for these users.

16 The building shall not be occupied until the areas shown on the approved plans for parking and manoeuvring of vehicles and cycles have been constructed and laid out in accordance with the approved plans, and thereafter such areas shall be retained solely for such purposes.

Reason: In the interests of highway safety.

17 The hours of construction, loading or unloading on the site shall be confined to 8:00 to 18:00 Monday to Friday, 9:00 to 13:00 Saturday and no working on Sundays or public holidays.

Reason: To protect the amenities of adjacent residents.

18 The covered and secure cycle parking facilities and storage facilities shall be provided in accordance with the approved plans prior to first occupation of the development hereby approved. The cycle storage shall have lockable doors and the internal space shall have facilities for locking cycles (using Sheffield type stands or similar). The cycle storage area and stands shall be retained thereafter.

Reason: In the interests of visual and residential amenity and to promote sustainable travel in accordance with paragraphs 108 and 127 of the NPPF.

19 The building shall not be occupied until the approved the bin storage area has been provided in accordance with the approved drawings and retained thereafter.

Reason To safeguard the residential amenity of neighbouring properties and to secure compliance with paragraph 127f) of the NPPF.

20 The site shall be developed with separate systems of drainage for foul and surface water on and off site.

Reason In the interest of satisfactory and sustainable drainage

21 Development shall not begin until details of foul and surface water drainage works have been submitted to and approved in writing by the Local Planning

Authority, and carried out in accordance with these approved details.

Reason: So that the Local Planning Authority may be satisfied with these details for the proper drainage of the site.

22 Prior to first occupation or use of the development hereby approved 'bat tubes' or other features providing a roosting crevice for bats must be constructed within the fabric of each building to the satisfaction of the Local Planning Authority

Reason: To take account of and enhance the biodiversity and wildlife interest of the area, and to be in accordance with Paragraph 175 of the NPPF (2018) to encourage the incorporation of biodiversity improvements in and around developments, especially where this can secure measurable net gains for biodiversity.

23 Prior to the commencement of development beyond foundation works full details of any external lighting including locations, design, numbers and luminance shall be submitted to and approved in writing by the Local Planning Authority. The development shall thenceforth be undertaken in strict accordance with the details thereby approved prior to first occupation of the units.

Reason To safeguard the biodiversity of the site and to secure compliance with Policy G12 of the Publication Draft City of York Local Plan 2018

24 Full details of the gated pedestrian and cycle access on to Racecourse Road illustrated on drawing reference 7988 P(000) 000 N shall be submitted to and approved in writing by the Local Planning Authority and the access provided prior to the first occupation of the development.

Reason: To ensure access to the development by sustainable means of transport and to secure compliance with paragraphs 108 and 110 of the NPPF.

8.0 INFORMATIVES:

Notes to Applicant

1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome:

i) Reduction in the scale and density of the development in order to better safeguard the setting of the adjacent Listed former factory

- ii) Sought a redesign of the proposed highway layout to improve its functionality
- iii) Sought an amendment of the housing mix to provide a greater range of units

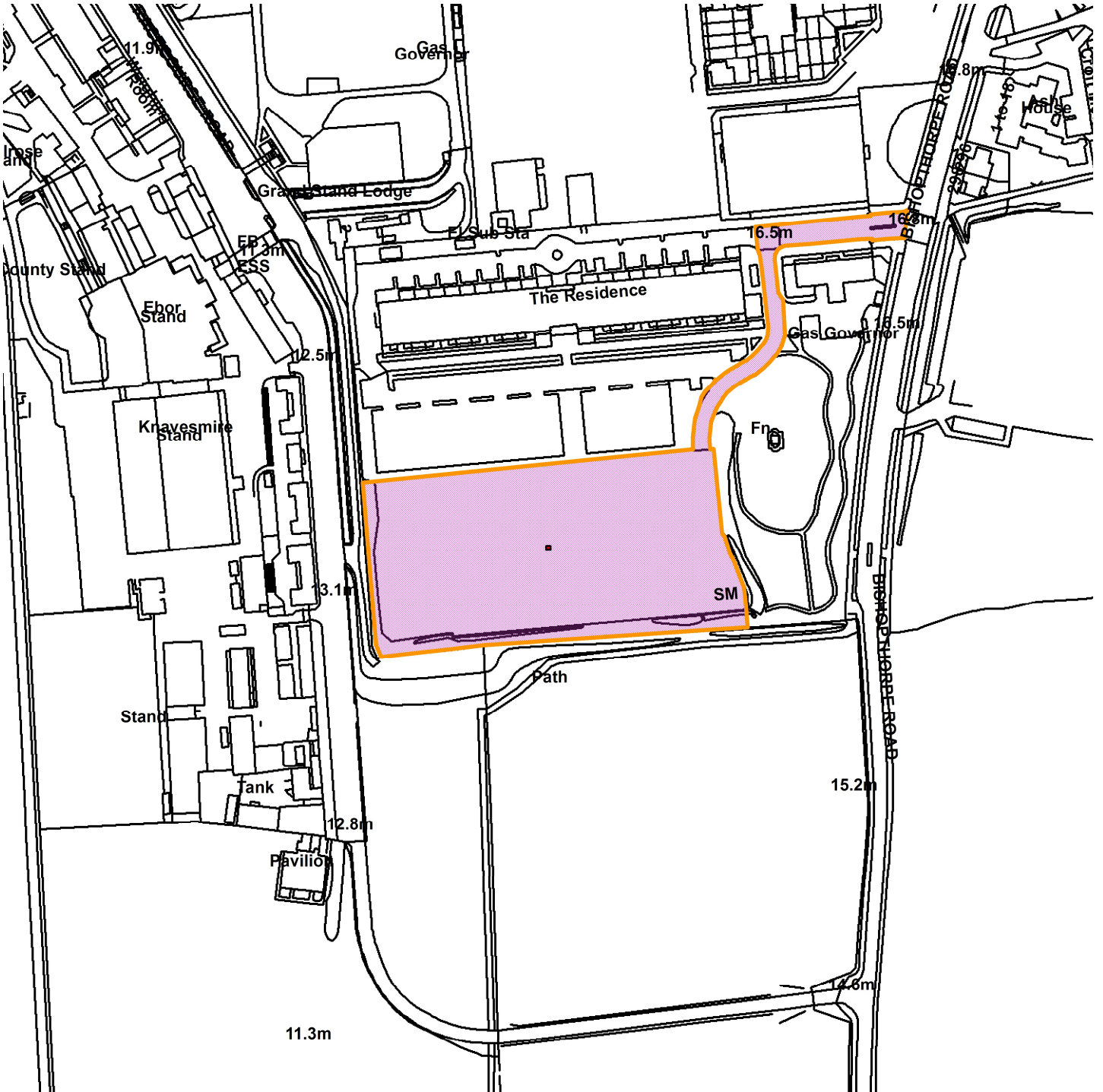
Contact details:

Case Officer: Erik Matthews

Tel No: 01904 551416

18/02582/FULM

Land South Of The Residence, Bishopthorpe Road, York



Scale : 1:2458

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plan
Date	30 July 2020
SLA Number	

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Planning Committee - Thursday 13 August 2020

18/02582/FULM

Land South Of The Residence

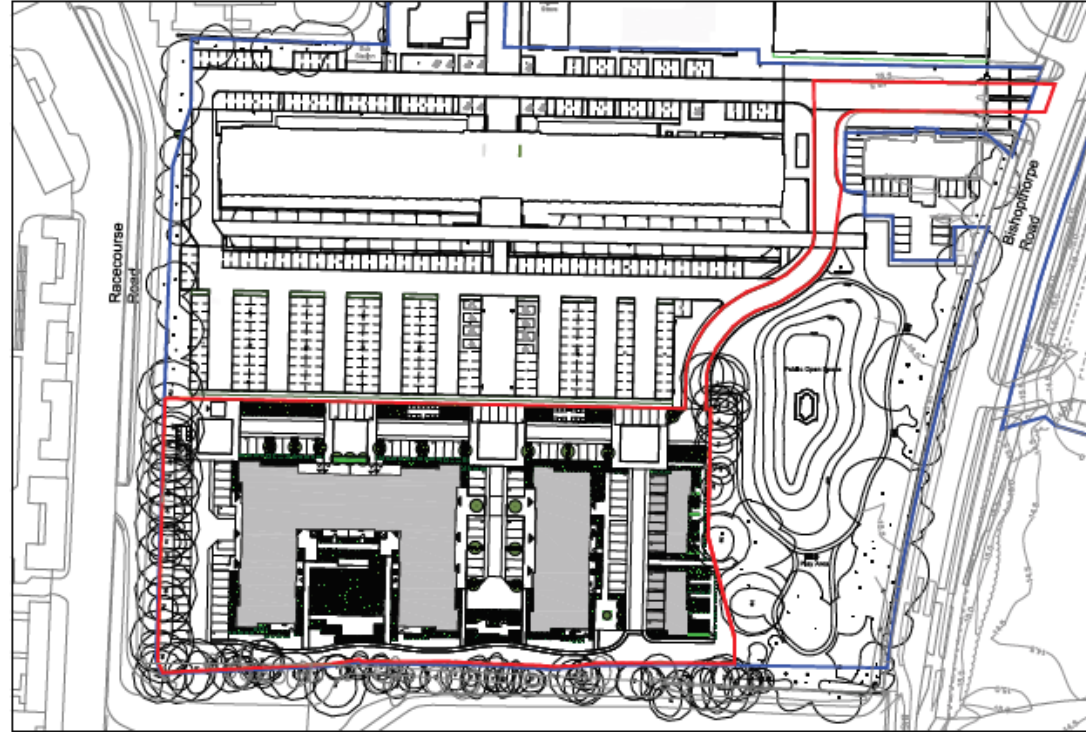
Bishopthorpe Road

View North along
Bishopthorpe Rd





City of York Council Planning Committee -Thursday 13 August
2020



N

B Updated in line with amended 06.04.20 DP CB scheme

A Updated in line with amended 22.01.20 DP CB scheme


revision	date	by	chk

All dimensions to be verified on site and the Architect informed of any discrepancy. All drawings and specifications should be read in conjunction with the Health and Safety Plan. All conflicts should be reported to the appointed Principal Designer.
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preliminary comment construction
 planning tender record

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drawn by	checked by	date	scale @ A3
DP	CB	05.11.2018	1:1250

**The Chocolate Works, York
Proposed Residential Development**

Site Location Plan

project number	drawing number	revision
7968	P(000)001	B



Refer to TPM Landscape's layout for detailed landscape design

Car Parking: 120no. spaces

Cycles: 132no. spaces

Indicative gated cycle and pedestrian link through Western boundary. Pending level information, land ownership confirmations and Council approval for connection to existing pathway beyond.

Construction of all services must allow for storm water management through appropriate drainage or storm and safety space saving cycle

Designation of cycle and pedestrian routes

Shared cycle routes

Pre-construction information

The designer will take the following health and safety risks relating to this drawing into account:

ref	description	severity	likelihood	risk	status
1	Construction of all services must allow for storm water management through appropriate drainage or storm and safety space saving cycle	High	Low	Medium	Open
2	Designation of cycle and pedestrian routes	Medium	Low	Low	Open
3	Shared cycle routes	Medium	Low	Low	Open
4	Pre-construction information	Medium	Low	Low	Open

Revision table:

no.	description	date	by	checked by
1	Issue for information	20.05.18	CB	CB
2	Issue for information	20.05.18	CB	CB
3	Issue for information	20.05.18	CB	CB
4	Issue for information	20.05.18	CB	CB
5	Issue for information	20.05.18	CB	CB
6	Issue for information	20.05.18	CB	CB
7	Issue for information	20.05.18	CB	CB
8	Issue for information	20.05.18	CB	CB
9	Issue for information	20.05.18	CB	CB
10	Issue for information	20.05.18	CB	CB
11	Issue for information	20.05.18	CB	CB
12	Issue for information	20.05.18	CB	CB
13	Issue for information	20.05.18	CB	CB
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18	Issue for information	20.05.18	CB	CB
19	Issue for information	20.05.18	CB	CB
20	Issue for information	20.05.18	CB	CB

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The Chocolate Works, York
Proposed Residential Development

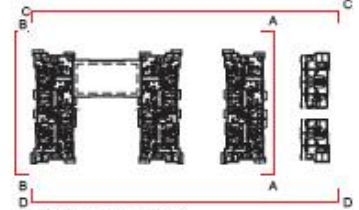
BOWMAN RILEY ARCHITECTS
Riverside Square, Riverside Street, Leeds LS1 2PL
0113 281 7878 www.bowmanriley.com

project number	drawing number	revision
7988	R/000/000	N

Proposed Elevations



Consultant staff and suppliers used on this project undertake the project through and with reference to all health and safety requirements of the design specification. This drawing shall be used as:
 - Design information for construction
 - Method statement
 - Risk assessment
 - For construction information
 The designer will not be responsible and will not be liable for any damage or loss incurred by using this drawing for any other purpose than the design process.



Reference Plan
1 : 1000

A	Update to include notes to City of York Council	08/04/20	DP	CB
B	Update to include notes to City of York Council	15/02/20	DP	CB
C	Update to include notes to City of York Council	02/02/19	DP	CB
D	Update to include notes to City of York Council	28/11/18	DP	CB
E	Finalise elevation to plan or finished	08/11/18	DP	CB
F	Drawings updated to include Council feedback for construction of parking	05/04/19	DP	CB
G	Revisions submitted per the clients request	02/04/19	WOC	CB
H	Revisions submitted to Council	02/04/19	WOC	CB
I	Drawings Update of new planning submission	02/11/2021	DP	CB
J	Update to include client meeting to include notes	23/10/21	DP	CB
K	Update to include redlines and Rev	23/10/2021	DP	CB

All documents to be read in conjunction with the latest version of any drawings. Drawings and specifications shall be used in conjunction with the Health and Safety File. All drawings shall be updated to the approved Project Design. The drawing is the property of Bowman Riley Architects Limited. © 2021 BRL. All Rights Reserved.

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Stonebridge Homes

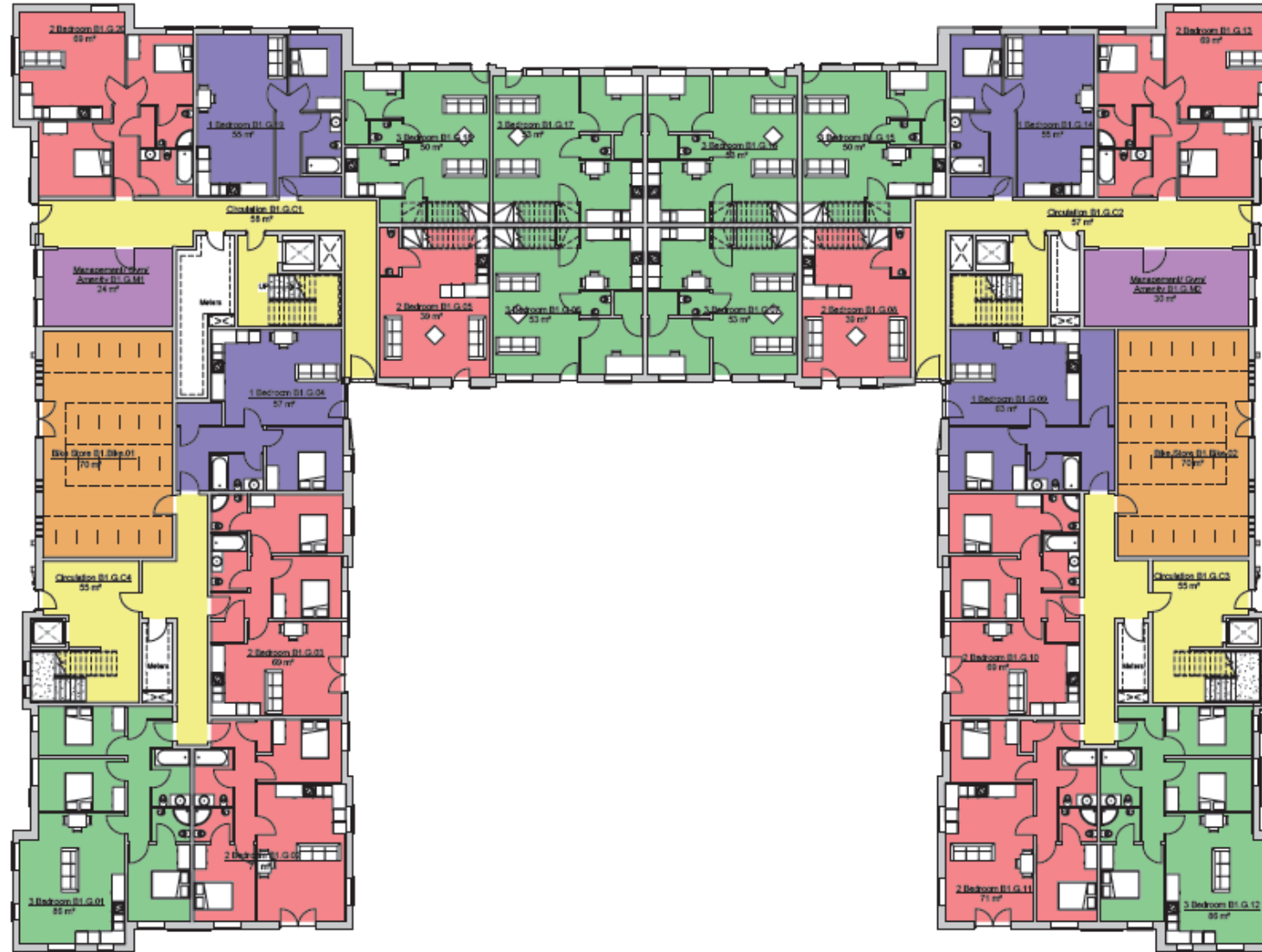
drawn by: checked by: date: scale @ A1
 DP: CB: 11.09.2016: As Indicated

The Chocolate Works York

Elevations

project number	drawing number	revision
7988	P(000)100	J

Block 1 Ground Floor Plan



Block 1 Ground Floor Plan
1 : 100

Consultation with the client and other stakeholders is required to ensure the proposed design meets the needs of all stakeholders and is in line with the relevant legislation. The design is subject to change and is not intended to be a final design. The design is subject to change and is not intended to be a final design.

ref	revised

Area Schedule Block 1 Ground Floor

Number	Type	Area
B1.G.01	3 Bedroom	86 m ²
B1.G.02	2 Bedroom	71 m ²
B1.G.03	2 Bedroom	69 m ²
B1.G.04	1 Bedroom	57 m ²
B1.G.05	2 Bedroom	39 m ²
B1.G.05	2 Bedroom	51 m ²
B1.G.06	3 Bedroom	53 m ²
B1.G.06	3 Bedroom	53 m ²
B1.G.07	3 Bedroom	53 m ²
B1.G.07	3 Bedroom	53 m ²
B1.G.08	2 Bedroom	39 m ²
B1.G.08	2 Bedroom	51 m ²
B1.G.09	1 Bedroom	63 m ²
B1.G.10	2 Bedroom	69 m ²
B1.G.11	2 Bedroom	71 m ²
B1.G.12	3 Bedroom	86 m ²
B1.G.13	2 Bedroom	69 m ²
B1.G.14	1 Bedroom	55 m ²
B1.G.15	3 Bedroom	50 m ²
B1.G.15	3 Bedroom	52 m ²
B1.G.16	3 Bedroom	53 m ²
B1.G.16	3 Bedroom	53 m ²
B1.G.17	3 Bedroom	53 m ²
B1.G.17	3 Bedroom	53 m ²
B1.G.18	3 Bedroom	50 m ²
B1.G.18	3 Bedroom	52 m ²
B1.G.19	1 Bedroom	55 m ²
B1.G.20	2 Bedroom	69 m ²
Totals	28	1626 m²

1. Update/Issued/Revised from Client	20.02.18
2. Update/Issued/Revised from City of York Council	26.11.18
3. Update/Issued/Revised from Planning	26.02.18
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98. Update/Issued/Revised from Planning	06.03.18
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100. Update/Issued/Revised from Planning	06.03.18

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Stonebridge Homes

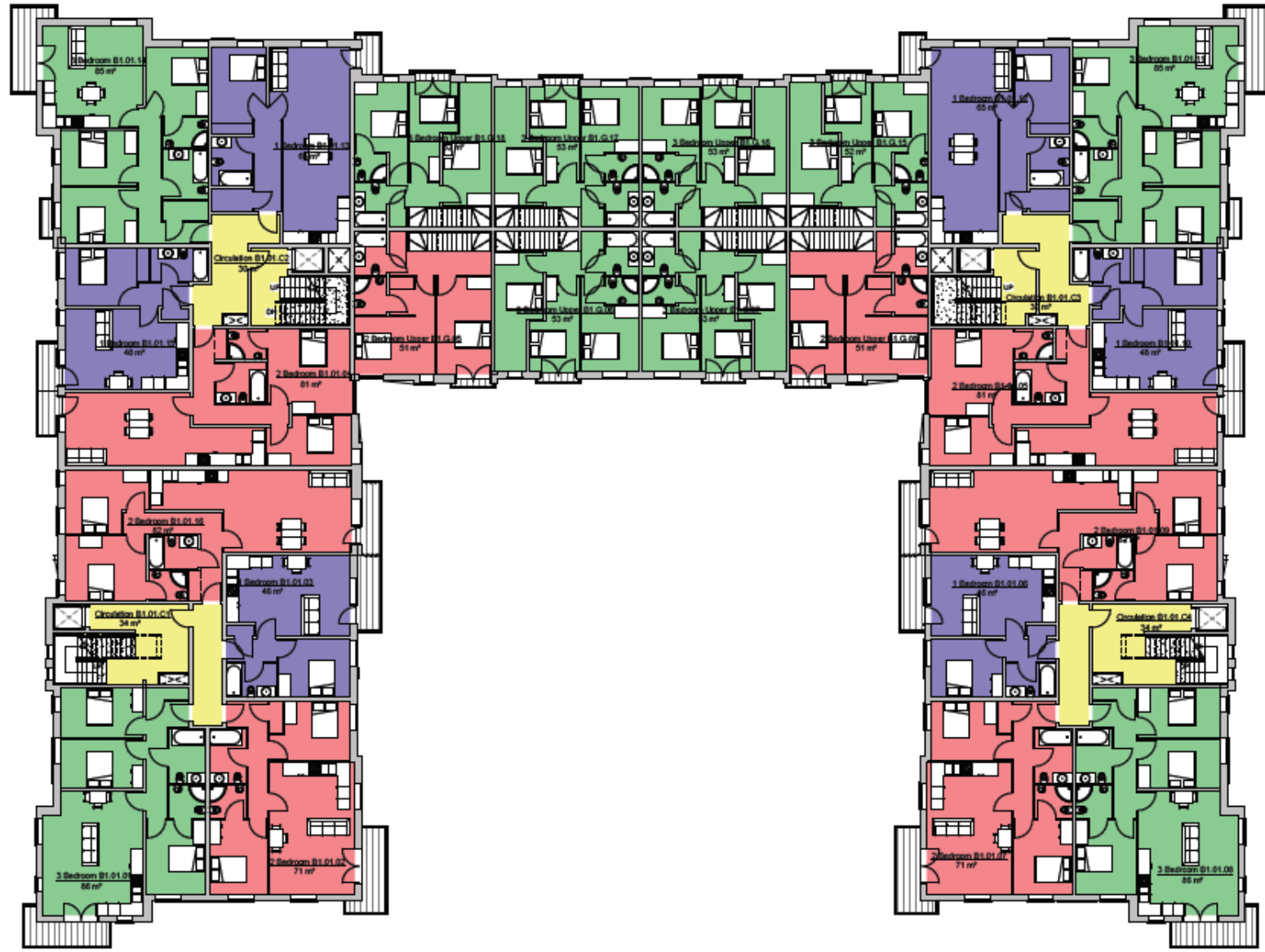
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DP	CB	30/07/18	1 : 100

The Chocolate Works York

Block 1 Ground Floor

project number	drawing number	revision
7588	P(000)010	G

Block 1 First Floor Plan



Block 1 First Floor Plan
1 : 100

Contractor shall be responsible and ensure the high contractor has provided through and approved information on all health and safety aspects relating to the design described in this drawing before the start of construction of the above works.

The design is not to be used for construction until the design has been checked during the design process.

Area Schedule Block 1 First Floor		
Number	Type	Area
B1.01.01	3 Bedroom	96 m ²
B1.01.02	2 Bedroom	71 m ²
B1.01.03	1 Bedroom	46 m ²
B1.01.04	2 Bedroom	81 m ²
B1.01.05	2 Bedroom	81 m ²
B1.01.06	1 Bedroom	46 m ²
B1.01.07	2 Bedroom	71 m ²
B1.01.08	3 Bedroom	96 m ²
B1.01.09	2 Bedroom	81 m ²
B1.01.10	1 Bedroom	46 m ²
B1.01.11	3 Bedroom	96 m ²
B1.01.12	1 Bedroom	46 m ²
B1.01.13	1 Bedroom	46 m ²
B1.01.14	3 Bedroom	96 m ²
B1.01.15	1 Bedroom	46 m ²
B1.01.16	2 Bedroom	81 m ²

1	Update the residential use of City of York Council	06/01	
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3	Update the residential use of City of York Council	26/11	DP
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99	Update the residential use of City of York Council	20/04	DP
100	Update the residential use of City of York Council	20/04	DP

All dimensions shall be in millimetres, and the Architect's consent of any alterations to the design shall be in writing and shall be subject to the approval of the City of York Council and the City of York Council Planning Department.

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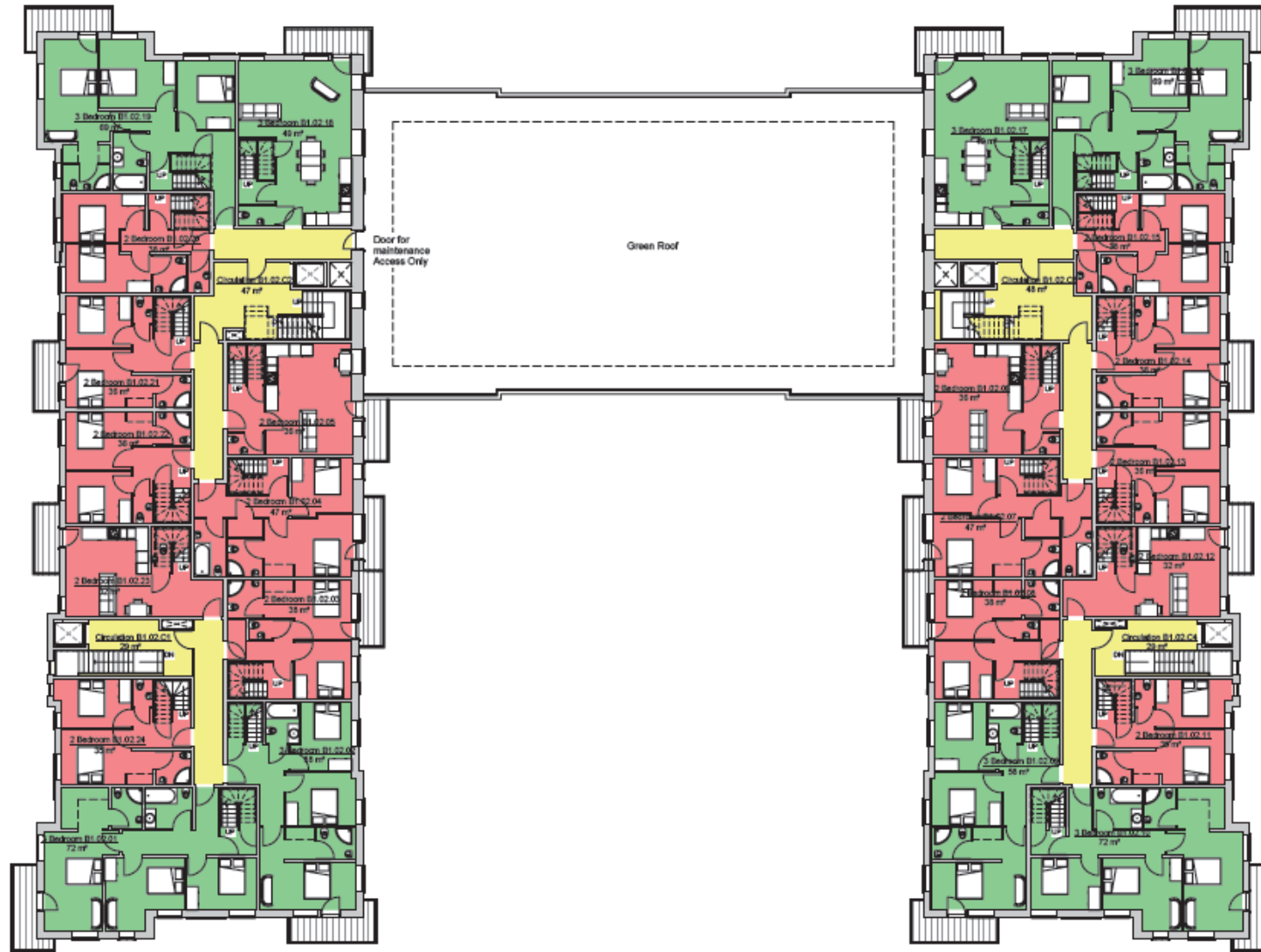
Stonebridge
Homes

drawn by	checked by	date	scale @ A1
DP	CB	30/07/18	1 : 100

The Chocolate Works
York

project number	drawing number	revision
7988	P(000)011	1

Block 1 Second Floor Plan



Area Schedule Block 1 Second Floor		
Number	Type	Area
B1.02.01	3 Bedroom	72 m ²
B1.02.01	3 Bedroom Upper	46 m ²
B1.02.02	3 Bedroom	55 m ²
B1.02.02	3 Bedroom Upper	49 m ²
B1.02.03	2 Bedroom	36 m ²
B1.02.03	2 Bedroom Upper	36 m ²
B1.02.04	2 Bedroom	47 m ²
B1.02.04	2 Bedroom Upper	36 m ²
B1.02.05	2 Bedroom	36 m ²
B1.02.05	2 Bedroom Upper	34 m ²
B1.02.06	2 Bedroom	36 m ²
B1.02.06	2 Bedroom Upper	34 m ²
B1.02.07	2 Bedroom	47 m ²
B1.02.07	2 Bedroom Upper	36 m ²
B1.02.08	2 Bedroom	36 m ²
B1.02.08	2 Bedroom Upper	36 m ²
B1.02.09	3 Bedroom	55 m ²
B1.02.09	3 Bedroom Upper	49 m ²
B1.02.10	3 Bedroom	72 m ²
B1.02.10	3 Bedroom Upper	46 m ²
B1.02.11	2 Bedroom	35 m ²
B1.02.11	2 Bedroom Upper	37 m ²
B1.02.12	2 Bedroom	32 m ²
B1.02.12	2 Bedroom Upper	55 m ²
B1.02.13	2 Bedroom	36 m ²
B1.02.13	2 Bedroom Upper	40 m ²
B1.02.14	2 Bedroom	36 m ²

Area Schedule Block 1 Second Floor		
Number	Type	Area
B1.02.14	2 Bedroom	42 m ²
B1.02.15	2 Bedroom	36 m ²
B1.02.15	2 Bedroom	36 m ²
B1.02.16	3 Bedroom	69 m ²
B1.02.16	3 Bedroom Upper	42 m ²
B1.02.17	3 Bedroom	49 m ²
B1.02.17	3 Bedroom Upper	49 m ²
B1.02.18	3 Bedroom	49 m ²
B1.02.18	3 Bedroom Upper	49 m ²
B1.02.19	3 Bedroom	69 m ²
B1.02.19	3 Bedroom Upper	42 m ²
B1.02.20	2 Bedroom	36 m ²
B1.02.20	2 Bedroom Upper	36 m ²
B1.02.21	2 Bedroom	36 m ²
B1.02.21	2 Bedroom Upper	42 m ²
B1.02.22	2 Bedroom	36 m ²
B1.02.22	2 Bedroom Upper	40 m ²
B1.02.23	2 Bedroom	52 m ²
B1.02.23	2 Bedroom Upper	56 m ²
B1.02.24	2 Bedroom	36 m ²
B1.02.24	2 Bedroom Upper	37 m ²

1. Applied for residential in City of York Council 10.09.20 CB
 2. Applied for residential in City of York Council 20.10.20 CB
 3. Applied for residential in City of York Council 20.11.20 CB
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 13. Local authority planning permission 20.11.20 CB
 14. Local authority planning permission 20.11.20 CB

revision	date	by
1	20/10/20	CB
2	20/10/20	CB

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stonebridge Homes

drawn by	checked by	date	scale @ A1
DP	CB	24/08/2018	1:100

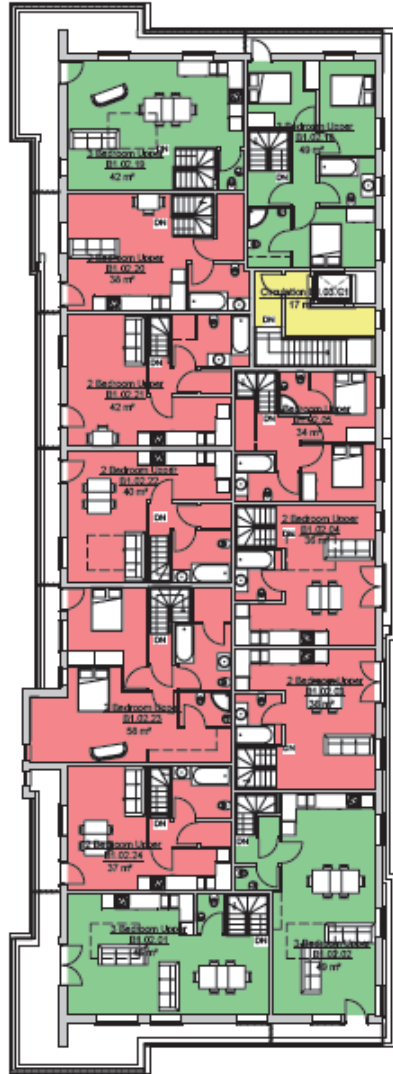
The Chocolate Works
York

Block 1 Second Floor

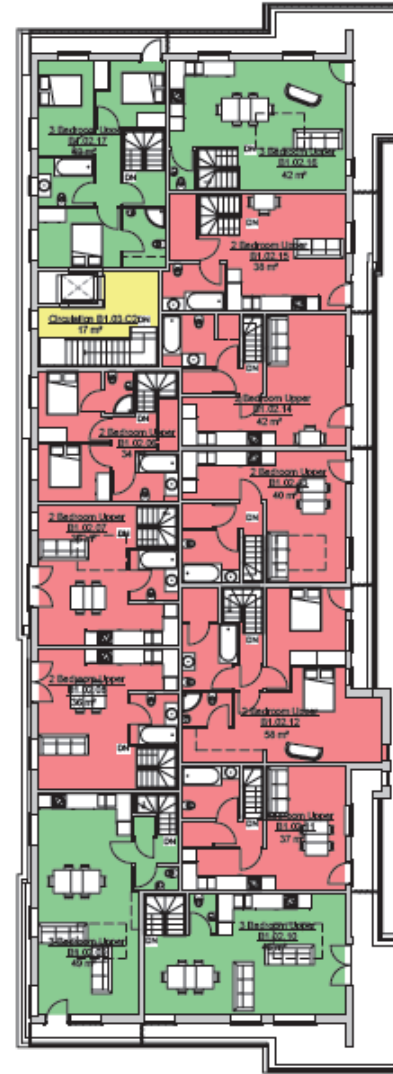
project number	drawing number	revision
7988	P(000)012	1

Block 1 Second Floor Plan
1:100

Block 1 Third Floor Plan



Block 1 Third Floor
1 : 100



Contractor shall ensure that the project complies with the provisions of the Building Regulations and all health and safety requirements, including the Design Structure Code, during the construction phase.

The designer is not liable for the following health and safety risks during the design phase:

ref	residual risk

J	Updated for submission to City of York Council	18.08.20	CB
I	Updated drawing for submission to City of York Council	20.12.19	CB
H	Updated for submission to City of York Council	20.11.19	CB
G	Updated for submission to City of York Council	20.11.19	CB
F	Updated for submission to City of York Council	20.11.19	CB
E	Updated for submission to City of York Council	20.11.19	CB
D	Updated for submission to City of York Council	20.11.19	CB
C	Updated for submission to City of York Council	20.11.19	CB
B	Updated for submission to City of York Council	20.11.19	CB
A	Updated for submission to City of York Council	20.11.19	CB

preliminary comment contribution
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 London: 1, The Arcade, York YO1 1AA
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Stonebridge
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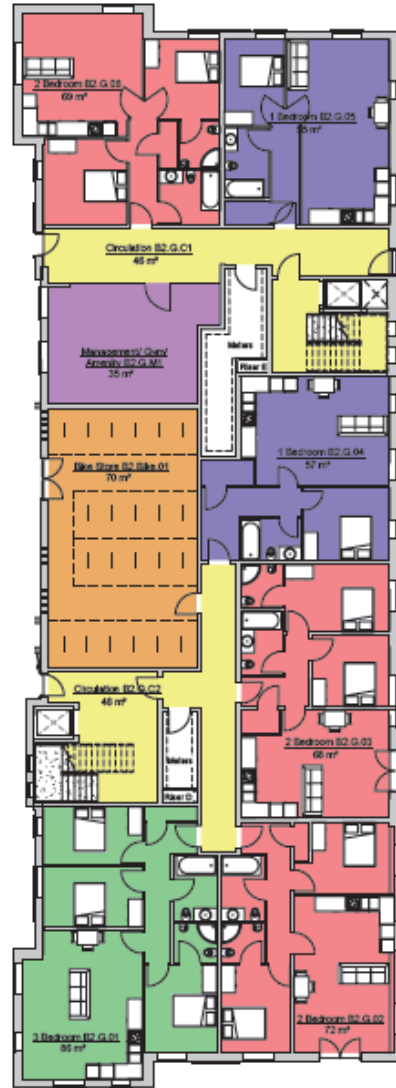
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The Chocolate Works
York

Block 1 Third Floor

project number	drawing number	revision
7988	P(000)013	J

Block 2 Ground Floor Plan



Block 2 Ground Floor plan
1 : 100

Construction self and specialist must ensure all plans include the provided drawings and specifications on all levels and safety systems relating to the design and construction. The design must include the following:

- Design and construction details
- Details to meet
- Fire and smoke protection
- Details to meet

The design must also include the following health and safety details relating to the design and construction during the design process.

ref	revision
-----	----------

Area Schedule Block 2 Ground Floor

Number	Type	Area
B2.G.01	3 Bedroom	88 m ²
B2.G.02	2 Bedroom	72 m ²
B2.G.03	2 Bedroom	68 m ²
B2.G.04	1 Bedroom	57 m ²
B2.G.05	1 Bedroom	55 m ²
B2.G.06	2 Bedroom	69 m ²
Totals:	6	407 m ²

revision	date	by
A	20/07/18	CB
B	20/07/18	CB
C	20/07/18	CB
D	20/07/18	CB
E	20/07/18	CB
F	20/07/18	CB

All drawings to be checked and approved by the architect and the client. The architect is responsible for the design and construction of the building and the client is responsible for the construction and the building. The architect is responsible for the design and construction of the building and the client is responsible for the construction and the building.

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DP	CB	30/07/18	1 : 100

The Chocolate Works
York

Block 2 Ground Floor

project number	drawing number	revision
7888	P(000)014	F

Block 2 First Floor Plan



Block 2 First Floor Plan
1 : 100

Construction and specifications must ensure that the high water table is protected through the use of appropriate and all other relevant aspects relating to the design details in this drawing include the notes at:

- Designation
- Material
- Finish
- Protection
- Installation

The design is for the following work and shall not include any other work not shown or described in the design process.

Area Schedule Block 2 First Floor		
Number	Type	Area
B2.01.01	3 Bedroom	66 m ²
B2.01.02	3 Bedroom	112 m ²
B2.01.03	2 Bedroom	50 m ²
B2.01.04	1 Bedroom	50 m ²
B2.01.05	3 Bedroom	66 m ²
B2.01.06	1 Bedroom	48 m ²
B2.01.07	2 Bedroom	50 m ²

- A. Update the main plan to City of York Council 2018.10.10 CB
- B. Update the main plan to City of York Council 2018.11.10 CB
- C. Update the main plan to City of York Council 2018.11.10 CB
- D. Update the main plan to City of York Council 2018.11.10 CB
- E. Update the main plan to City of York Council 2018.11.10 CB
- F. Update the main plan to City of York Council 2018.11.10 CB
- G. Update the main plan to City of York Council 2018.11.10 CB
- H. Update the main plan to City of York Council 2018.11.10 CB
- I. Update the main plan to City of York Council 2018.11.10 CB
- J. Update the main plan to City of York Council 2018.11.10 CB
- K. Update the main plan to City of York Council 2018.11.10 CB
- L. Update the main plan to City of York Council 2018.11.10 CB
- M. Update the main plan to City of York Council 2018.11.10 CB
- N. Update the main plan to City of York Council 2018.11.10 CB
- O. Update the main plan to City of York Council 2018.11.10 CB
- P. Update the main plan to City of York Council 2018.11.10 CB
- Q. Update the main plan to City of York Council 2018.11.10 CB
- R. Update the main plan to City of York Council 2018.11.10 CB
- S. Update the main plan to City of York Council 2018.11.10 CB
- T. Update the main plan to City of York Council 2018.11.10 CB
- U. Update the main plan to City of York Council 2018.11.10 CB
- V. Update the main plan to City of York Council 2018.11.10 CB
- W. Update the main plan to City of York Council 2018.11.10 CB
- X. Update the main plan to City of York Council 2018.11.10 CB
- Y. Update the main plan to City of York Council 2018.11.10 CB
- Z. Update the main plan to City of York Council 2018.11.10 CB

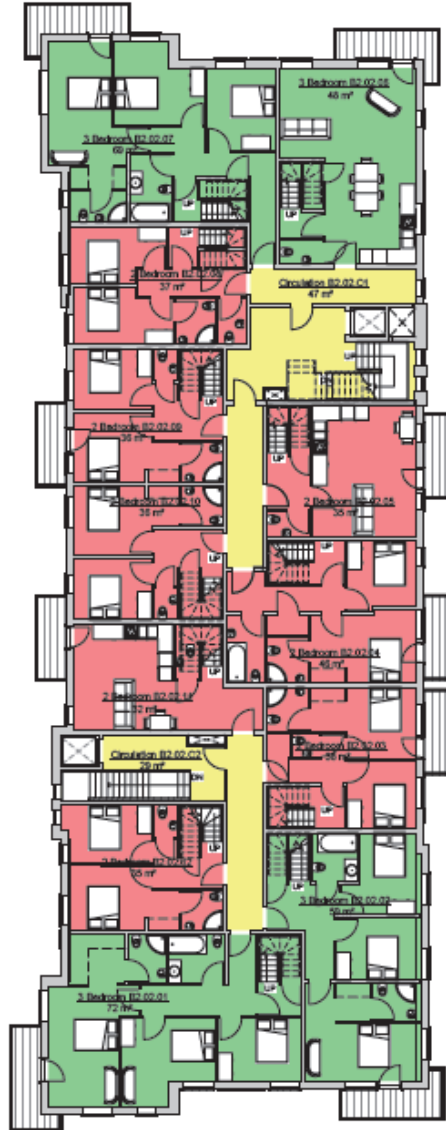
All dimensions to be within 10mm, and the finished ground level of any excavation to be within 10mm of the actual ground level. All dimensions to be within 10mm of the actual ground level. All dimensions to be within 10mm of the actual ground level.

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DP	CB	30/07/18	1 : 100
The Chocolate Works York			
Block 2 First Floor			
project number	drawing number	revision	
7988	P(000)015	H	

Block 2 Second Floor Plan



Block 2 Second Floor Plan
1 : 100

Area Schedule Block 2 Second Floor		
Number	Type	Area
B2.02.01	3 Bedroom	72 m ²
B2.02.01	3 Bedroom Upper	46 m ²
B2.02.02	3 Bedroom	59 m ²
B2.02.02	3 Bedroom Upper	49 m ²
B2.02.03	2 Bedroom	36 m ²
B2.02.03	2 Bedroom Upper	36 m ²
B2.02.04	2 Bedroom	46 m ²
B2.02.04	2 Bedroom Upper	36 m ²
B2.02.05	2 Bedroom	35 m ²
B2.02.05	2 Bedroom Upper	34 m ²
B2.02.06	3 Bedroom	49 m ²
B2.02.06	3 Bedroom Upper	49 m ²
B2.02.07	3 Bedroom	69 m ²
B2.02.07	3 Bedroom Upper	42 m ²
B2.02.08	2 Bedroom	37 m ²
B2.02.08	2 Bedroom Upper	39 m ²
B2.02.09	2 Bedroom	36 m ²
B2.02.09	2 Bedroom Upper	42 m ²
B2.02.10	2 Bedroom	36 m ²
B2.02.10	2 Bedroom Upper	40 m ²
B2.02.11	2 Bedroom	32 m ²
B2.02.11	2 Bedroom Upper	59 m ²
B2.02.12	2 Bedroom	35 m ²
B2.02.12	2 Bedroom Upper	37 m ²
Totals		1050 m²

Contributor shall ensure that all operators and users are fully trained in the use of the equipment and that all safety procedures are followed. The contractor shall ensure that all safety procedures are followed. The contractor shall ensure that all safety procedures are followed. The contractor shall ensure that all safety procedures are followed.

revision	date	by	ch
A	24/08/2016	DP	CB
B	24/08/2016	DP	CB

All dimensions to be verified on site. The contractor shall ensure that all dimensions are correct. The contractor shall ensure that all dimensions are correct. The contractor shall ensure that all dimensions are correct.

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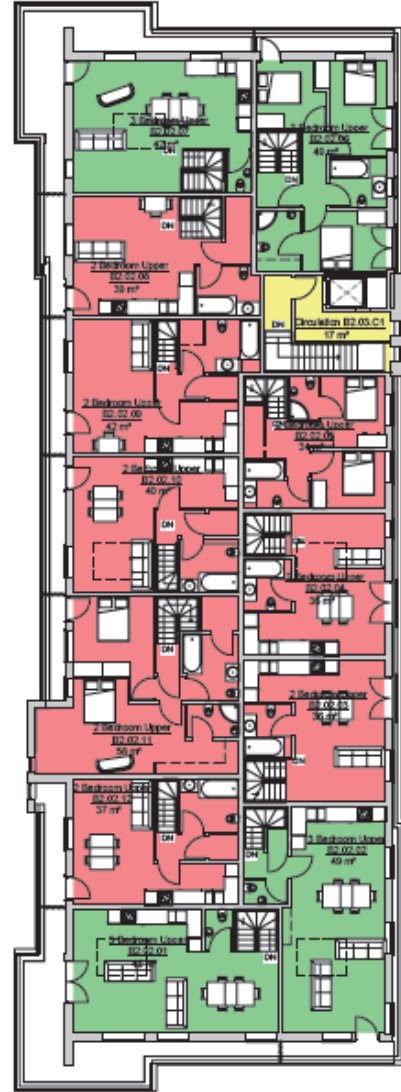
drawn by	checked by	date	scale
DP	CB	24/08/2016	1 : 100

The Chocolate Works
York

Block 2 Second Floor

project number	drawing number	revision
7588	P(000)016	G

Block 2 Third Floor Plan



Block 2 Third Floor
1 : 100

<p>Contractor shall ensure that all quantities and areas are checked and verified for accuracy and any discrepancies are reported immediately to the architect.</p> <p>Changes to the design shall be made in accordance with the following:</p> <ul style="list-style-type: none"> Changes to the design shall be made in accordance with the following: Changes to the design shall be made in accordance with the following: 	
ref	revision date

1	Update the submission to City of York Council	26.11.18	DP	CB
2	Layout update to planning	28.10.18	DP	CB
3	Changes update to York City Council	28.10.18	DP	CB
4	Update to planning	28.10.18	DP	CB
5	ADVA updated to include the new	08.04.19	HC	CB
6	Changes updated from planning	20.11.2018	DP	CB
7	Update planning and lighting	20.11.2018	DP	CB
8	Update planning and lighting	20.11.2018	DP	CB
9	Update planning and lighting	20.11.2018	DP	CB
10	Update planning and lighting	20.11.2018	DP	CB
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100	Update planning and lighting	20.11.2018	DP	CB

All dimensions to be checked on site, and the Architect is not responsible for any discrepancies between the drawings and the actual construction. It is the responsibility of the Contractor to ensure that the drawings are in accordance with the Building Regulations and any other applicable legislation.

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Stonebridge Homes

drawn by: DP checked by: CB date: 30/07/18 scale @ A1

The Chocolate Works York

Block 2 Third Floor

project number	drawing number	revision
7588	P(000)017	H

File reference: P17588 - Chocolate Works/7588 - 07 Rev01/3 Shaped Block Planning Model_CSB17588.rvt

Townhouse (Blocks 3 and 4) Elevations



① Elevation 1
1 : 100



② Elevation 2
1 : 100



③ Elevation 3
1 : 100



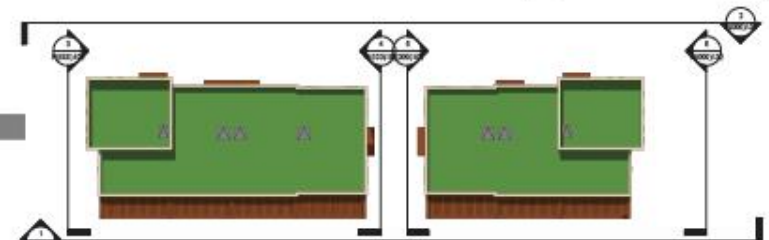
④ Elevation 4
1 : 100



⑤ Elevation 5
1 : 100



⑥ Elevation 6
1 : 100



Reference Plan
1 : 200

Construction and specification details to be provided and approved by the Council prior to construction. The drawings are for information only and do not constitute a contract. The drawings are for information only and do not constitute a contract. The drawings are for information only and do not constitute a contract.

C. Applied for resolution in City of York Council	08.03.19	DP	CB
A. Applied for resolution in City of York Council	28.11.18	DP	CB
A. Applied for submission to planning	28.11.18	DP	CB
revision	date	by	checked

All dimensions to be unless stated in title and the technical content of any drawings. Drawings and specifications shall be used in conjunction with the Contract and Bill of Materials. All work shall be completed to the satisfaction of the Council.

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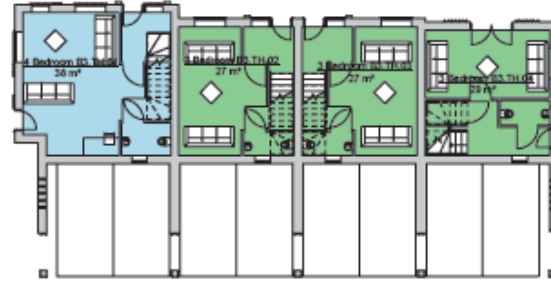
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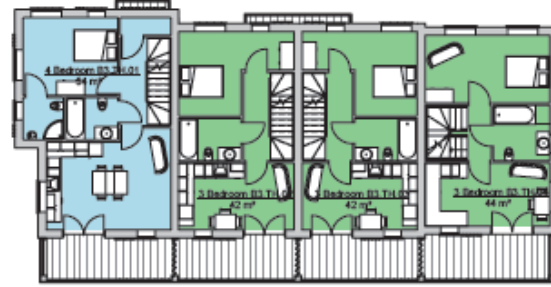
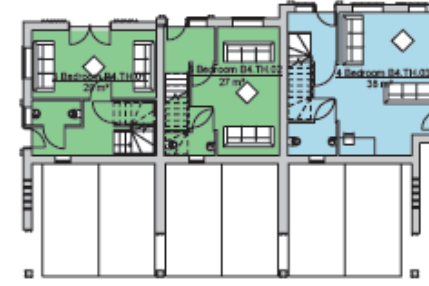
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drawn by	checked by	date	scale @ A1
DP	CB	05.11.19	As Indicated
The Chocolate Works York			
Blocks 3 & 4 Elevations			
project number	drawing number	revision	
7988	P(000)102	C	

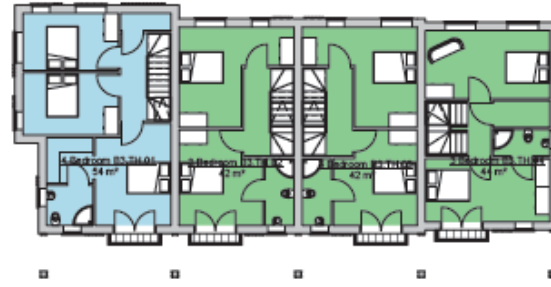
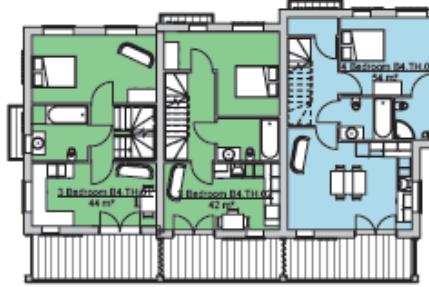
Townhouse Floor plans



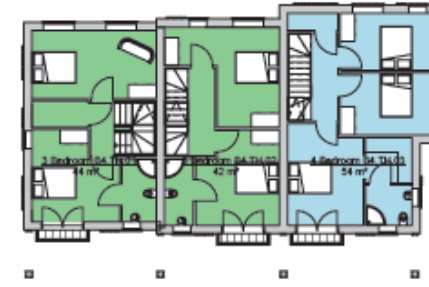
Ground Floor
1 : 100



First Floor
1 : 100



Second Floor
1 : 100



Consultation with the client and other stakeholders has provided the necessary information to ensure the design is safe and sound. The design is based on the information provided and is subject to change without notice. The design is based on the information provided and is subject to change without notice. The design is based on the information provided and is subject to change without notice.

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Number	Name	Area
B3.TH.01	4 Bedroom	146 m ²
B3.TH.02	3 Bedroom	111 m ²
B3.TH.03	3 Bedroom	111 m ²
B3.TH.04	3 Bedroom	116 m ²
B4.TH.01	3 Bedroom	116 m ²
B4.TH.02	3 Bedroom	111 m ²
B4.TH.03	4 Bedroom	146 m ²
7 total		

All information in the above schedule shall be taken as approximate and not a guarantee. All measurements are based on the information provided and are subject to change without notice.

revision	date	by
A	28.11.2024	CB
B	28.11.2024	CB
C	05.02.2025	CB
D	20.02.2025	CB
E	05.03.2025	CB

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Stonebridge
Homes

drawn by	checked by	date	scale @ A1
DP	CB	31.03.20	1 : 100

The Chocolate Works
York

Townhouse Floor Plans

project number	drawing number	revision
7988	P (000) 050	E

COMMITTEE REPORT

Date: 13 August 2020 **Ward:** Clifton
Team: West Area **Parish:** Clifton Planning Panel

Reference: 19/00246/FULM
Application at: York City Football Club Bootham Crescent York YO30 7AQ
For: Erection of 93 dwellings with associated access, infrastructure, landscaping, public open space and parking.
By: Persimmon Homes Limited And York City Football Club
Application Type: Major Full Application
Target Date: 1 August 2020
Recommendation: Approve subject to Section 106 Agreement

1.0 PROPOSAL**APPLICATION SITE**

1.1 The application relates to Bootham Crescent football ground. The football ground opened in 1932. Prior to this the site was used as a cricket ground. Local field patterns and land ownership dictated the shape of the site.

1.2 The site is allocated for housing in the Publication Draft Local Plan 2018 (2018 DLP). The estimated yield for the site is 86 dwellings, delivered in years 1-10 of the plan. There is agreement with the applicants that development will only commence when the new community stadium is ready for use (and there would be a planning condition to this effect).

1.3 The site is within the urban area and is surrounded by the following land uses -

- Predominantly 19th century terraced housing to the north and east and to the south along St Olaves Road.
- More substantial villas along St Peters Grove; within the Clifton Conservation Area.
- Duncombe Barracks to the west (the Council has purchased the northern portion of the site and intends to develop the site for housing).
- Recent predominantly 3-storey housing along Grosvenor Road (application submitted 2001).
- Former Shipton Street school to north, converted in residential (application initially approved 2007, varied 2009); this includes housing which sits close to the north boundary.

PROPOSALS

1.4 The scheme has been revised on two occasions, in December 2019 and March 2020. Both schemes were subject to re-consultation.

1.5 The revised scheme is for 93 dwellings (with 20% affordable housing) and includes a central public open space.

1.6 The scheme has been developed in consultation with Historic England and representatives from the football club (Historic England have been working on a project about the club and the ground titled Sharing Memories, Shaping Place). It consequently incorporates the following elements that will give distinctive character to the development and evidence the site's past use -

- A memorial garden and a retained section of the west stand. The retained section of terrace along with evidence of the location of the centre circle within the landscaping will allow for orientation and evidence of the previous layout of the site.
- The 'proposed flag location' annotated on the site plan relates to the flag present at the football ground (in a similar location). Historically the flag was lowered gradually towards the end of the game.
- The west brick boundary wall, which predates use of the site by the football club will be retained (it will be lowered removing the blockwork).

1.7 The site access would be from Grosvenor Road and a footpath instated where the south stand presently abuts the road. Housing would face onto Grosvenor Road and run alongside the east and north sides of the site. In the centre of the site, housing would be focused on the public open space. The public open space would be evident from the site entrance, and it will incorporate children's play. The layout enables future connectivity, for pedestrians and cyclists, through the site towards Burton Stone Lane.

1.8 Dwellings would be 2-storey along the north and east sides of the site. At the south side, facing Grosvenor Road, housing is 3-storey. The apartments in the centre of the site would be 3.5 storey, the houses 3-storey.

1.9 There is at least 1 car parking space per dwelling, 2 spaces for each of the larger 4-bed dwellings, typically 1 space for others and 15 visitor parking spaces. The visitor spaces are located across the site.

1.10 The housing mix would be as follows –

	Total		Affordable
1-bed	12	13%	4

2-bed	33	36%	8
3-bed	37	40%	6
4-bed	11	12%	0

2.0 POLICY CONTEXT

2.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that determinations be made in accordance with the development plan unless material considerations indicate otherwise.

2.2 The National Planning Policy Framework ('NPPF') is a material consideration in the determination of this planning application. Key policies / sections of the NPPF are as follows –

Section 4 Decision making
 Section 5 Delivering a sufficient supply of homes
 Section 11 Making effective use of land
 Section 12 Achieving well-designed places

2.3 The Publication Draft City of York Local Plan 2018 (2018 DLP) was submitted for examination on 25 May 2018. Phase 1 of the hearings into the examination of the Local Plan took place in December 2019. In accordance with paragraph 48 of the NPPF the Draft Plan policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012.

2.4 Key relevant Publication Draft Local Plan 2018 Policies are as follows -

SS1 Delivering Sustainable Growth for York
 H1 Housing Allocations
 H2 Density of Residential Development
 H3 Balancing the Housing Market
 H10 Affordable Housing
 HW7 Healthy Places
 D1 Place-making
 D2 Landscape and Setting
 D6 Archaeology
 GI6 New Open Space Provision

CC1 Renewable and Low Carbon Energy Generation and Storage
CC2 Sustainable Design and Construction of New Development
ENV1 Air Quality
ENV3 Land Contamination
ENV5 Sustainable Drainage
T1 Sustainable Access

3.0 CONSULTATIONS

INTERNAL

ARCHAEOLOGY

3.1 In addition to commenting on below ground archaeology, officers have been working with Historic England with regards the social value and heritage of the football ground.

3.2 An archaeological building recording of the football ground will be required. Particular attention should be paid to the original stands which date to the 1930s and any evidence of the use of the tunnel at the back of the Popular Stand as an air raid shelter. The ground as a whole should be captured by photographic record and all original features should be recorded. A forthcoming audit of features within the ground will also help inform this part of the work. A laser scan of football ground from the centre sport is also required as part of the recording condition. This will enable future digital projects to re-create the ground virtually using an app.

3.3 The site lies outside of the Central Area of Archaeological Importance. However, the modern roads of Bootham and Clifton have Roman origins with Clifton having several known areas of Roman occupation and cemeteries. There is the potential for Roman and later archaeological features/deposits to exist across the site in lesser disturbed areas.

3.4 Given that any below-ground archaeology that may survive on the site is likely to be of local/regional significance and may only exist in isolated pockets evaluation work (trenching) can be conditioned. Evaluation trenching may lead to further excavation. If required this should be undertaken with an element of community engagement/involvement. There may be several modern cremation cask burials across the site particularly in the northern half of the site, these will be investigated during the evaluation phase.

3.5 In terms of the work Historic England have commissioned, this includes work addressing intangible heritage i.e. oral histories, memories, photographs relating to

the use of the site as the home of York City FC throughout the 2018/19 season. As part of this project it was hoped that the memory of the football ground could be presented within the proposed development and the following ideas have been presented to the applicants –

Within the new development there will be the ability to orientate oneself within the demolished football ground (digitally and physically). This will also be an important element for any future digital projects which supporters groups may wish to apply for funding for in the future.

An area of memorial for the people currently commemorated at the ground by plaques, those who have had their ashes scattered or who have had their ashes buried at the ground.

The retention of the boundary wall as a marker of the former cricket and football ground and for potential use as an art space.

Some original elements of the ground (including some rare or even unique survivals) could be retained somehow within the new development. For example, part of the tunnel which runs under the Popular Stand. This may involve a short section of the tunnel (locked off to public)/concrete steps of stand above it being retained, surrounded or alongside a memorial garden area containing burials and cores from the pitch commemorating those who have had ashes scattered at the ground. This could be surrounded/fenced off with a section of the original white picket fence visible at the Pop Stand which formerly ran round the entire pitch.

CONSERVATION ARCHITECT

3.6 Officers recommendations were as follows –

- The Council has acquired the site to the west to develop for housing. A link to this site and onto Burton Stone Lane will be an essential part of this application, to connect the anticipated open spaces on each side of the boundary so that they have a positive relationship as well as creating a legible and well-designed pedestrian / cycle route between the two developments.
- Further detailing of landscaping will be required, to explain how different areas will be used/typologies of open space, where there will be defendable space to apartments (which could be achieved by boundary treatment or planting). The space around the flag pole has the potential to create a high quality entrance into the development, but this isn't currently realised.
- Vehicle parking should be integrated so visually screened by landscaping. Parking courts should remain small and frontage parking not dominant. All parking should have natural surveillance.

- Building design - the elevations need detailing which better reflects the character of York. Suggestions include removal of “stuck on” porch canopies, introduction of door cases (typical of York), and reducing window height at upper floors.
- It was recommended the apartments be orientated so their principle outlook was south towards the open place and this has been accommodated in the latest scheme.

LANDSCAPE ARCHITECT

3.7 Further detail of the landscaping will be required to understand the structure and principles. The proposed areas present an opportunity for a significant, multifunctional, shared space defined with trees, hedging and other planting, providing structures for play, and potentially other outdoor community uses such as growing produce and outdoor eating.

ECOLOGY

3.8 No objections provided conditions are used to protect bats and provide compensatory habitat.

3.9 A low level of bat activity was recorded along the western boundary (which is lined by trees). During the 2018 emergence survey a bat was observed leaving a crevice within the east stand. A licence from Natural England will be required before demolition works. To mitigate for the loss of bat roost facilities, mitigation is recommended - bat bricks and bat boxes that would be located to the western side of the site, on housing and in the trees by the boundary.

3.10 The east stand currently supports an individual or very small number of transitional roosting Common Pipistrelle bats which are common and widespread throughout the UK and classed as a species of 'least' conservation concern. The requirement for a European Protected Species Licence will prevent any direct harm. The provision of six bat bricks and six bat boxes will maintain the long-term roosting opportunities on site.

EDUCATION

3.11 Officers have asked for the ability to review whether a pre-school contribution would be required prior to commencement. Projections confirm there is currently, and would be, sufficient capacity at primary, secondary and early years if the scheme is built out by 2024/25 and there is no reduction in existing schools and providers. The early years and childcare market is very fluid therefore officers would like to review this again once a build date is confirmed to determine capacity of providers at that point in time. Should the position have changed and current

providers not be able to accommodate the additional places, the maximum request would be for a contribution for 11 early years places to support expansion at existing provision. This would equate to a contribution of £100,309.

HIGHWAY NETWORK MANAGEMENT

3.12 The Transport Assessment, using an acceptable modelling approach (TRICS database), anticipates under 30 extra cars trips as a consequence of the development during peak hours. Based on previous national guidance on Transport Assessments such an impact would not require an assessment on the wider network. Current national guidance is less specific and sets out the number of factors which require consideration, these include (relevant to this application) cumulative impact and availability of public transport. The intention is to reduce the number of car trips (by 10%) through a travel plan promoting sustainable travel. Due to the site allocation for residential development, the low level of associated anticipated traffic, and considering its size and location and measures to promote sustainable travel, an assessment of impact on the wider network is not justified.

3.13 With regards the proposed main street, it is considered due to the amount of car parking served from the area which will have a shared surface, this is potentially an acceptable approach (the York highway design guide recommends such spaces are appropriate when they serve up to 50 dwellings). However as set out in the design guide, the shared areas must be designed so they are distinctively different from other estate road, recommended is different surfacing and a distinctive gateway at the entrance to mark the transition.

3.14 Guidance has been provided on dimensions for parking areas. The preference is for car parking spaces to be longer than 4.8 m where perpendicular to the road to prevent cars overhanging roads and footpaths. Visitor parking is recommended at a rate of 1 space per 4 dwellings.

3.15 Layout recommendations -

- Avoid excessive runs of dropped kerb on site frontages so streets are not car dominated. This was specifically raised in relation to the site frontage and in-front of the apartments.
- Pedestrian / cycle routes should be 3 m wide.
- It was asked for bin storage areas (for collection) to be provided and cycle stores to be reasonably close to apartments.

HOUSING

3.16 Officers have negotiated and are agreed on the proposed affordable housing scheme, considering the tenure, location of dwellings and house types. It is noted that the provision needs to be rounded up to 20% (it is 19.4%) and this can be

secured through an off-site contribution. It is asked that the contribution be based on the value of the 4-bed dwellings because provision should be pro-rata (although adjusted accordingly where required to meet overall need).

LEISURE

3.17 Officers have provided details of where of site contributions could be used and these are as follows –

- Bootham Allotments
- Bootham Park –playing fields
- Clarence Gardens bowling club
- Crombie Avenue – teenagers play and sport facilities
- Sports pitches by Burton Green primary school (subject to further investigation and agreement from the school over the use of ancillary facilities)
- Clifton Park and Homestead Park

PUBLIC PROTECTION

Construction

3.18 Request a Construction Management Plan for minimising the creation of noise, vibration and dust during the demolition, site preparation and construction phases due to surrounding houses.

Noise

3.19 A condition is requested that the housing be provided with the noise specification as recommended in the submitted noise assessment.

Contaminated Land

3.20 Recommend a site investigation and remediation. The site investigation issued identified that the soil and made ground including the earth embankment at the north of the site is potentially contaminated. The roofs of the buildings currently on site are likely to include at least in part, cement bound asbestos sheeting. The risk is assessed as being moderate / low for the future residential users.

Electric Vehicle Charging

3.21 When parking spaces are adjacent to dwellings/in-curtilage officers provision for electric vehicle charging. Elsewhere request a minimum of 5% active and 5% passive provision. Passive provision will require consideration of future power requirements for such points, allowance for any necessary cabling and for groundwork to be installed from the outset. The locations and specifications of charge points would be subject to approval/

EXTERNAL

CLIFTON WARD PLANNING PANEL

3.22 Make the following comment on design –

- Suggest respecting the existing building line along Grosvenor Terrace (which would enable car parking at the front).
- Ask that brickwork respects the locality.
- Advocate inclusion of secure by design principles
- Suggest that the open space should be well enclosed and central rather than adjacent the side boundary of the barracks due to noise from the firing range, that could be unsettling.

ENVIRONMENT AGENCY

3.23 No objection. Have advised on standing advice with regards land contamination, waste and flood risk.

HISTORIC ENGLAND

3.24 The revised plan is a considerable improvement of the first planning layout and reflects the discussions held on site with the various interested parties as to the significance of the place in heritage terms and the wishes of the fans and local residents.

Retained section of Popular Stand terrace and tunnel

3.25 The retained terrace and tunnel will provide a lasting legacy of the stadium and create a focal point for memory and orientation. The location of the retained terrace and tunnel matches the desired position on the halfway line at the midpoint of the Popular Stand and in front of the POS. The precise length of the section will be determined by conservation, engineering and health and safety considerations but is not expected to exceed 6m.

3.26 The preferred location for the memorial garden is around the base of this structure to provide discreet location for remembrance. The side walls of the terrace could be used to support memorial plaques etc, while caskets and ashes could be buried at the base of the walls.

Some existing metal fencing and gates in the Popular Stand could be appropriated to secure the perimeter at the top of the terrace and ends of the tunnel. Similarly, the wooden picket fence in front of the Popular Stand should be reclaimed to border the memorial garden.

‘Centre’ circle

3.27 The idea of recreating the centre circle in the middle of the POS is applauded, it would be in alignment with the retained section of terrace and provide a further place for orientation.

Flagpole

3.28 The flagpole was originally located between the south-east corner of the pitch and the stadium entrance. It is suggested that the new flagpole is erected as close as possible to this original location, and that it flies a replica of the club flag as a permanent and symbolic reminder of fans' allegiance to Bootham Crescent. Its proposed location does not exactly match the original position, but it is as near as possible in the proposed layout. Ideally, like the centre circle, it should be slightly further south and east, closer to the new entrance.

Orientation and plan form

3.29 The concept of providing a central built form, with town houses and apartments framing a central POS with gaps between, imaginatively gives the feel of a 'stadium nestled between terraced housing'. As recommended by Historic England the layout of housing in the central area has been aligned to the west boundary wall.

Boundary wall

3.30 The brick boundary wall will be retained but where concrete blockwork has been added to the summit this should be removed. Where necessary, consolidation of the boundary wall should be undertaken using reclaimed bricks from elsewhere on the site. Consideration should be given to retaining the walls of the gents urinal built against the west boundary wall south of the Popular Stand and reusing the structure for storage of maintenance equipment and/or to screen new infrastructure. Any further structures bonded to the boundary wall, and which are not being retained, should have their wall scars consolidated or refaced as buttresses. The face of the west boundary wall, either side of the retained terrace could accommodate old signage from the ground and/or appropriate artwork.

SECURE BY DESIGN OFFICER (NORTH YORKSHIRE POLICE)

3.31 Officers' comment on the original scheme (now superseded) was as detailed below.

- Management and maintenance of public open space required.
- Children's play area – recommend these are a reasonable distance from housing to prevent issues around noise and anti-social behaviour. 20m from habitable rooms recommended.

- Clearly defined public and private space and public space that has good natural surveillance recommended.
- Access control into private rear gardens required.
- Parking provision – advise against remote parking due to security issues and potential for indiscriminate parking that can lead to tension. Any courtyard parking should be intimate in scale (5 spaces maximum recommended) – such parking arrangements can have adverse effect on communities and place-making as people tend not to use their front doors. On the revised scheme officers considered this issue been addressed as the shared parking areas had been made smaller, spaces were closer to homes they would serve and natural surveillance of the areas had been introduced as the flats above garages had been added.
- Provision of secure cycle storage for each dwelling is necessary to promote sustainable travel.

SPORT ENGLAND

3.32 Do not object to the loss of the sports ground as it is being re-provided as part of the community stadium scheme (along with leisure facilities further artificial pitches). Sport England require a mechanism to require the community stadium is ready for use before re-development of Bootham Crescent commences.

YORK CIVIC TRUST

3.33 York Civic Trust is sympathetic to a residential reuse of the site, providing much-needed accommodation in line with York's housing requirements. However the trust objected to the original scheme on the following grounds:

- An unimaginative aesthetic and lack of connectivity with the wider area.
- An absence of any meaningful acknowledgement of (football) heritage in the scheme.
- The claimed noise attenuation benefits (due to the barracks) are not convincing.
- Increased traffic in the area.

YORKSHIRE WATER

3.34 As sub-soil conditions do not support the use of soakaways accept that surface water may discharge to the public surface water sewer in Grosvenor Road at 30 litres/second as suggested in the Flood Risk Assessment. Ask for separate systems for foul and surface water run-off.

4.0 REPRESENTATIONS

4.1 Comments from Local Councillors Myres and Wells are below. Cllrs were consulted on the revised plans on 25.3.2020.

- Support use of the site for housing
- The houses proposed look standard and do not reflect or enhance local distinctiveness.
- Affordable housing targets are inadequate and an exceedance of a policy compliant 20% should be provided. Housing should be available to those on average salaries and local residents.
- Open space- The plans should include a play area of sufficient size for the local community. The play area must include wheelchair accessible equipment. It should not be segregated; there should be access from the north and the west of the site.
- Ask for the development to respect the sporting heritage and for the developers to work with Historic England in this respect.
- There is a lack of sporting amenity in the area, it may be fitting to work with the Management Committee of the Burton Lane Club to establish whether improved facilities for local people, such as darts, snooker, dominoes and other leisure facilities can be provided through developer contributions. Currently, there is a weekly gathering for older people in the football ground at Bootham Crescent; we need to consider how older persons activities can continue to be provided in the area, and again, the Club may be a good location for this.
- Traffic – concern existing congestion on Burton Stone Lane, in particular junctions with Bootham and Crichton Avenue will be exacerbated. It is asked whether off site works can improve these junctions.
- There should be enhanced car club and car share in this area, where there are parking issues – which are made worse due to the proximity of the hospital and city centre.
- Drainage – lack of capacity in existing infrastructure

Publicity and Neighbour Notification

4.2 There has been multiple rounds of public consultation. The most recent consultation was in April 2020, when significant changes were as follows - house types were changed at the site frontage, the apartments were increased to 3.5 storey and plots 49 and 50 were moved further from the site boundary. There have been 9 contributors. Comments as a consequence of consultation made the following points -

Principle of the proposed development

- Objection to the loss of the football ground which is a community asset and a preference for the site to remain as open space.
- Housing proposed is unimaginative and does not respect local character.
- Housing facing Grosvenor Road, due to its terraced design and 3 storey scale would be unrelenting and un-welcoming. Having these houses set further back or rotated so they face east-west was recommended.
- The area cannot cope that the extra traffic that would result from the scheme (Bootham and Burton Stone Lane cited). This would also have an adverse effect on air quality.
- Impact on demand for car parking on surrounding streets due to lack of car parking provision.
- The scheme should have better / more evident sustainability credentials – use of renewable energy and tree planting for example.
- Inability of existing drainage infrastructure to accommodate more development.

Impact on neighbours' amenity

- Concerns the houses would be over-bearing and cause overlooking over houses on St Olaves Road to the south and Bootham Green to the north, specifically due to the house type and location of houses on plots 15-17 and 49 and how they will overlook neighbouring housing and gardens.
- Comments were made about noise from traffic due to the internal layout and proximity of the road to the site boundary and Bootham Green to the north. These has been addressed in the revised plans as houses back onto the site boundary on the northern and eastern sides.

Concern about damage to neighbouring properties during demolition.

5.0 APPRAISAL

KEY ISSUES

5.1 The key issues are as follows –

- Principle of the proposed use
- Dwelling mix
- Design
- Residential amenity
- Highway network management and safety
- Drainage / Flood Risk
- Public Protection

- Sustainable design and construction
- Archaeology
- Biodiversity
- Education
- Open Space
- Community uses / open space

ASSESSMENT

PRINCIPLE OF THE PROPOSED USE.

5.2 This is a development where the presumption in favour of development applies, as set out in paragraph 11 of the NPPF. This means that permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole.

5.3 The site is in the urban area and is due to become vacant; at such a time its re-use for housing will be consistent with the policies in the NPPF with regarding to housing delivery and making efficient use of previously developed land in sustainable locations. In particular NPPF paragraph 118 states planning decisions should "give substantial weight to the value of using suitable brownfield land within settlements for homes and other identified needs".

5.4 The site is allocated for housing in the Publication Draft Local Plan 2018 (2018 DLP). The proposal is reasonably consistent with the allocation; the proposed amount of development is 93 dwellings (71 dwellings per ha) compared to the anticipated yield 86 of dwellings.

DWELLING MIX

5.5 The density of development is broadly compliant with 2018 DLP policy H2. H2 establishes target densities of 100 units per hectare in the city centre and 50 in the urban area. The policy advises target densities can be adjusted to relate to local context and character and that higher densities can be supported within 400m of high frequency public transport corridors (this site is under 400 m from Bootham which is a frequent route). Paragraphs 122 and 123 of the NPPF promotes building at a high density, making optimum use of sites, especially those in sustainable locations, whilst providing well-designed, attractive and healthy places.

5.6 Policy H3 of the 2018 DLP states proposals for residential development will be required to balance the housing market by including a mix of types of housing, which reflects the diverse mix of need across the city. This includes flats and smaller houses for those accessing the housing market for the first time, family housing of 2 to 3 beds and homes with features attractive to older people. The Local Plan policies are city-wide and it is expected that housing density, type and mix will differ from site to site. Policies H2 and H3 can be given moderate weight only in accordance with NPPF policy as set out in paragraph 2.3 above.

5.7 The scheme proposes 71 dwellings per hectare, predominantly housing and predominantly 2 (36%) and 3 bed (40%) sized dwellings. The density and mix is appropriate considering recommended densities, the sustainable location, respecting local density and urban grain and local housing need.

Affordable Housing

5.8 2018 DLP policy H10 establishes local targets for affordable housing. For Brownfield sites this is 20% overall, with 80% for rent, 20% discount sale. The policy in so far as it relates to major developments (for 10 or more dwellings) can be given moderate weight in decision making.

5.9 The affordable housing provision would be 20% overall, the mix is as set out below. The 18 affordable dwellings are pepper-potted through the site, and will be “tenure blind” as preferred by policy H10. Whilst the mix is not strictly pro-rata, this is at officers’ request as it provides a mix that better reflects need (as set out in the SHMA 2016 addendum. The on-site provision is 19.4%. To round up the 0.6 of a dwelling shortfall a contribution will be sought towards off site provision.

Proposed affordable housing compared against local need as detailed in the SHMA addendum

Dwelling size	1-bed	2-bed	3-bed	4-bed
Need as per SHMA	35-40%	30-35%	20-25%	5-10%
Proposed	22%	44%	33%	0%

DESIGN

5.10 NPPF paragraph 127 states that planning decisions should ensure that developments:

- a) will function well and add to the overall quality of the area;
- b) are visually attractive as a result of good architecture, layout and landscaping;
- c) are sympathetic to local character and history, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- d) establish or maintain a strong sense of place;
- e) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space); and
- f) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

Site layout

5.11 In arriving at the proposed layout the applicants have engaged with Historic England and the scheme incorporates the following key elements in line with their recommendations (these have been considered by the applicants in terms of future maintenance, and health and safety) –

- A retained section of the west stand, adjacent the half-way line, and the passageway behind. Historically fans would use the tunnel swap ends (behind the goals) at half time.
- Memorial space
- The flag-pole (historically the flag was lowered towards the end of the game) retained within the public open space, close to its original location.
- The brick boundary wall on the western side of the site (blockwork removed).

5.12 The scheme is also sympathetic with the emerging proposals for redevelopment of the barracks site to the west. In this respect, it will allow for a pedestrian and cycle route, 3 m wide through the central public open space within both sites. This will provide good permeability not only for this site, in terms of access to amenities but for the wider community.

5.13 The layout follows good practice in terms of urban design by providing better connectivity for pedestrians and cyclists, and how it respects the urban grain; along the south, east and north sides of the sites housing will respect the surrounding pattern of development - layout, building lines, scale of buildings and local separation distances. The public open space (POS) will be central - a well-overlooked focal point. The POS will be visually apparent from the site entrance and as the site will not be a cul-de-sac for pedestrians, this will function as a facility for the community.

Public realm / landscaping

5.14 The specific landscape design of the POS will be secured through condition. It will allude to the heritage and history of the site, the components of which have been determined in consultation with Historic England. The public will be able to orientate themselves with the former layout due to the retention of part of the west stand and the incorporation of the former centre circle, annotated within the public open space. In addition to retained physical features on site there will be a laser scan of the ground carried out which will be publically accessible and allow orientation (an understanding of how the ground appeared in the past). The football club will be involved with the management of the memorial area and consequently retain a degree of ownership over this space.

Car parking

5.15 The majority of parking spaces will be in communal areas. Normally such large parking areas as those proposed here would be avoided (in favour of in-curtilage parking for houses) but they are acceptable in this case as they will be reasonably discreet; screened from public view by the proposed housing and boundary treatment and landscaping. Visually parked cars will not dominate the public realm. Each dwelling has at least 1 allocated space and there are 15 visitor parking spaces.

Housing design

5.16 The details and materials of the housing will be secured through condition. The intention is to respect the locality in terms of brickwork and the houses facing Grosvenor Road will have slate tiled roofs, as this is prevalent and distinctive in the area. The apartments in the centre of the site have been amended in response to officer comments. They primarily overlook the open space and are dual aspect, as recommended in National Design guidance, to provide good living conditions.

RESIDENTIAL AMENITY

5.17 The NPPF states that developments should create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

5.18 In terms of health & well-being all of the dwellings have their own private amenity space or have good accessibility to the POS. The dwellings will be spacious and have good outlook, all being dual aspect.

5.19 A condition will require secure by design principles are followed, this will cover issues such as access to communal spaces and stores and means of providing defensible space to the ground floor apartments. Public realm will have good natural surveillance. Parking areas, which have limited natural surveillance, are to be designed so they appear semi-private and are discreet from the public realm.

Relationship with surrounding houses

5.20 The proposed houses would not be over-bearing or over-dominant over surrounding houses. On the northern and eastern sides of the site there would be 2-storey housing at the periphery. In terms of separation distances rear elevation to rear elevation would vary between 18 m and 21 m. This is acceptable, compared against Newborough Street and Scarborough Terrace to the east of the site where rear-to-rear separation distances are around 16 m. The only exception to this is on plot 49 where the separation between housing would be just under 18 m. This is because the neighbouring housing at Bootham Green is so close to the site boundary (under 5 m at its closest point) and consequently can only expect to have limited outlook. Plot 49 would have a generous deep garden, over 12 m in length. The relationship would be acceptable under the circumstances. A condition can prevent upper floor rear extensions to plot 49 to maintain amenity.

5.21 The proposed layout would introduce openness on the western side of the site. Houses in the centre of the site will not unduly over-look or compromise redevelopment of the barracks site to the west.

5.22 Houses on the south side of the site would continue the general scale and alignment of existing housing along Grosvenor Road. The houses would be some 16 m from gardens on the opposite side of the street, specifically 66 St Olaves Road, who have objected to the proposed arrangement. Since the objection, the house types facing the garden have been changed and Juliet balconies removed from the front elevation. The possibility for some views into a garden on the opposite side of the road, from bedroom windows is considered reasonable in this urban context.

HIGHWAY NETWORK MANAGEMENT

5.23 The NPPF states that in assessing applications it should be ensured that:

- Opportunities to promote, and facilitate access to sustainable transport included where appropriate.
- Give priority first to pedestrian and cycle movements.
- Minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards.
- Be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations.
- Safe and suitable access to the site be achieved for all users.
- Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree. Paragraph 111 requires an assessment of impacts on the network, when development will generate significant amounts of movement.

5.24 The NFFPF also states “Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

5.25 With regards place-making and highway design, the National Design Guide gives the following advice and this will be realised within this scheme – “Successful development depends upon a movement network that makes connections to destinations, places and communities, both within the site and beyond its boundaries”.

Impact on the network

5.26 As stated in the NPPF in terms of impact on the highway network refusal can only be sustained if the impact would be “severe”. This site has been allocated for housing in the local plan. The overall plan has been developed to accommodate growth over the plan period; its transport policies are consistent with the strategic themes of the City of York Council Local Transport Plan (2011-2031) that are (relevant to this scheme) to:

- provide quality alternatives (to the car)
- provide strategic links
- implement behavioural change
- tackle transport emissions

5.27 The Transport Assessment, using an acceptable modelling approach (TRICS database), anticipates under 30 extra cars trips as a consequence of the development during peak hours. Based on previous national guidance on Transport Assessments such an impact would not require an assessment on the wider

network. Current national guidance is less specific and sets out the number of factors which require consideration, these include (relevant to this application) cumulative impact and availability of public transport. The intention is to reduce the number of car trips (by 10%) through a travel plan promoting sustainable travel. Due to the site allocation for residential development, the low level of associated anticipated traffic, and considering its size and location and measures to promote sustainable travel, an assessment of impact on the wider network is not justified.

5.28 There is at least one car parking space per dwelling on site. All but two of the houses with three or more bedrooms have two car parking spaces and there are 15 visitor parking spaces. This is generous provision for an inner city site such as this. Officers have not opposed this approach as - the only local parking standards are in the 2005 draft local plan, and as the NPPF states maximum standards should only be imposed if “there is a clear and compelling justification that they are necessary for managing the local road network, or for optimising the density of development in city and town centres and other locations that are well served by public transport”. Cycle storage is shown on plan for houses without their own outside amenity space i.e. the apartments. There are two covered and secure spaces for each apartment. It is unlikely the development will lead to overspill parking causing safety issues on surrounding streets. There is a car club space on the street opposite (Newborough Street) and future occupants will be encouraged/incentivised to use this scheme. There are no grounds to resist the application in terms of impact on the highway network, in particular as this is an allocated housing site in a sustainable location.

5.29 The main street is designed to accommodate servicing and emergency access. Bin stores / collection points are illustrated on plan for the apartments and houses towards the centre of the site.

The promotion of sustainable travel

5.30 The application accords with the NPPF requirement to give priority to pedestrians and cyclists and National Design Guide advice on place-making, on the following grounds -

- Footpath instated past the site along Grosvenor Road, therefore improving existing conditions.
- A connection toward Burton Stone Lane for pedestrians and cyclists only facilitated. This will improve the network for future residents and the wider community in terms of providing a more direct route to amenities, work and public transport.
- Most of the estate road within the site will be designed as a shared space, following the hierarchy for users established in the NPPF. The detailed design

will be subject to input from highways officers being mindful of road safety and will incorporate soft landscaping where practical.

- A travel plan that intends to reduce the anticipated number of private vehicle trips by 10%.

DRAINAGE

5.31 Relevant NPPF advice on flood risk is not to increase flood risk elsewhere. With regards drainage developments should incorporate sustainable systems, unless there is clear evidence that such would be inappropriate. Local Policy ENV 5 requires surface water run off at brownfield sites to be restricted to 70% of the existing rate. Sustainable drainage, following the hierarchy in the NPPG, should be facilitated unless this is not feasible.

5.32 The site is in Flood Zone 1 where there is a low probability of flooding and where residential development is suitable.

5.33 Sustainable drainage principles for surface water have been applied, connection into surface water sewers is the preferred option as ground conditions do not allow for soakaways and it is not possible to connect directly into a watercourse.

5.34 In accordance with local guidance the surface water run-off will be 70% of the existing rate. A rate of 30 litres per second has been agreed by Yorkshire Water. A condition will secure site-specific details. It is noted an (underground) water pumping facility may be required associated with the drainage – this is annotated on plan to the north of the bin/cycle store for the apartments.

PUBLIC PROTECTION

5.35 NPPF policy on land contamination and noise is set out in section 15 of the NPPF. The site investigation recommends further investigation and potential remediation to ensure the site is suitable for future users.

Noise

5.36 A noise assessment has considered the current environment and has made recommendations to ensure the proposed houses meet World Health Organisation standards. This will be secured through condition in addition there will be a further condition to ensure any plant/machinery on site (if required) does not cause disturbance, as it is understood a water treatment facility is necessary for the housing.

Construction management

5.37 A Construction Management Plan for minimising the creation of noise, vibration and dust during the demolition, site preparation and construction phases will be required due to surrounding houses. This will be secured through condition. It will require adherence to best practice/best practical means, control times of construction, ensure the highway network is kept clean during works and ensure there is a community liaison officer / contact details should surrounding residents have concerns.

Air quality / EV charging

5.38 In line with City of York Council's Low Emission Strategy and paragraph 110 of the NPPF facilities for charging plug-in and other ultra low emission vehicles will be required. A condition is proposed that requires houses with in-curtilage parking have charging facilities. In other areas there will be 5% provision with a further 5% 'passive' provision to enable extra points in future. This is in accordance with current guidance from Public Protection.

Contaminated Land

5.39 A site investigation and remediation is recommended and can be secured through condition. The site investigation issued identified that the soil and made ground including the earth embankment at the north of the site is potentially contaminated.

SUSTAINABLE DESIGN AND CONSTRUCTION

5.40 A planning condition will require the buildings to be compliant with 2018 DLP policies CC 1 and 2 which requires exceedance of building regulations with regards to water and energy efficiency and carbon emissions (through the use of low/zero carbon technology and building fabric efficiency).

ARCHAEOLOGY

5.41 Policy D7 of 2018 DLP requires an understanding of archaeology affected, to avoid substantial harm (preserve 95% of deposits) or where there would be harm, undertake adequate mitigation.

5.42 In terms of above ground archaeology paragraph 199 of the NPPF asks for the recording of assets to be lost, to advance the understanding of the asset. The football ground will be subject to a recording prior to demolition works. For below ground archaeology investigation is also required, as explained in 3.4. The scheme of investigation for these works is agreed.

BIODIVERSITY

5.43 NPPF paragraph 170 states decisions should contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity. In respect of biodiversity surveys have detected bat activity at the site. Planning conditions will secure replacement habitat on site (to replace football stands) and confirmation from Natural England that prior to relevant demolition (the timber clad section of the east stand) either a license has been granted for the works or confirmation that such is not required.

EDUCATION

5.44 NPPF paragraph 94 states that it is important that a sufficient choice of school places is available to meet the needs of existing and new communities. Local planning authorities should take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education. They should give great weight to the need to create, expand or alter schools through the preparation of plans and decisions on applications". Local supplementary planning guidance explains how the need for extra education spaces are determined and the relevant planning obligations.

5.45 Currently primary and secondary schools in the locality can accommodate this development. There will be need for pre-school spaces. A financial contribution to secure pre-school facilities will be secured through s106 agreement.

OPEN SPACE

5.46 The NPPF advises that planning decision should aim to create healthy and inclusive places. Paragraph 96 states 'access to a network of high quality open spaces and opportunities for sport and physical activity is important for the health and well-being of communities. Planning policies should be based on robust and up-to-date assessments of the need for open space, sport and recreation facilities (including quantitative or qualitative deficits or surpluses) and opportunities for new provision. Information gained from the assessments should be used to determine what open space, sport and recreational provision is needed, which plans should then seek to accommodate'.

5.47 Policy GI6 (new open space provision) of the 2018 DLP states 'all residential development proposals should contribute to the provision of open space for recreation and amenity'... 'The precise type of on-site provision required will depend on the size and location of the proposal and the existing open space provision in the area. Where there are deficiencies in certain types of open space provision in the area surrounding a proposed development, the Council will seek variations in the component elements to be provided by the developer in order to help to overcome them'. The policy goes on to state that the Council will encourage on-site provision

where possible but off-site provision will be considered acceptable in certain circumstances.

5.48 Within this scheme is a significant amount of public open space (2,700sqm) central within the site. This will include children's play and amenity space, in addition to memorial space associated with the football club. Based upon demand generated from the anticipated number of future residents the amount of amenity space still falls short of the requirements within the 2017 open space and green infrastructure audit of 40.5 sq m per person/bedroom (9,437sqm overall). As such there will also be an off-site contribution which could be used at the play area at Crombie Avenue, Bootham allotments, Clifton and Homestead Park.

5.49 The intention is sports facilities are provided off-site. Officers have liaised with local councillors to identify local projects and the off-site sports contribution can be prioritised towards improvements to the facility at Crombie Avenue and the re-provision of sports pitches on the land around Burton Green School. There is also the potential to use funds at Bootham Park. The contributions towards off site children's play and sports facilities will be secured by a s106 agreement.

6.0 CONCLUSION

6.1 A presumption in favour of development applies at this site. The policy for decision making in the NPPF applies which states permission should be granted unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole.

6.2 The amount and type of development proposed for the site is acceptable and broadly compliant with the 2018 DLP allocation. Planning conditions and a s106 agreement can be used to secure reasonable compliance with national and local policies with regard to housing need, promoting sustainable transport, design and the impact on the environment.

6.3 The recommendation is that delegated authority be given to the Assistant Director of Planning and Public Protection to APPROVE the application subject to conditions and completion of a s106 agreement to secure the following obligations –

Affordable housing

20% - on-site as illustrated on the approved site plan and a contribution for off-site to round up the 0.6% shortfall.

Education

£100,309 for 11 early years places within 1.5km of the site

Open Space – sport

Application Reference Number: 19/00246/FULM

Item No: 4c

Off-site contribution of £49,629 to be used to enhance sports and play facilities at either Crombie Avenue, Burton Green, Clarence Gardens, or Bootham Park

Open space – amenity

£25,066 to be used at Bootham allotments, Crombie Avenue play area, Clifton Park or Homestead Park.

Highways

£5k for Traffic Regulation Orders to facilitate changes to Grosvenor Road.

Sustainable travel

£200 per dwelling for bus pass / cycle for first occupiers.

7.0 RECOMMENDATION: Approve subject to Section 106 Agreement

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans:-

Proposed site layout drawing 100 revision S

House types

Apartments - Aycliffe 3.5 storey - drawing AY-3.5-WD10

House types package dated 20.12.2019 - Aidan, Clayton Corner, Howard, Linton, Morden, Sutton

House types dated 29.1.2019 - FOG, Hanbury, Greyfairs, Wolvesey

Details of waste and cycle storage drawings 113C and BBWD10

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 Re-provision of community sports facility

The development hereby permitted shall not be commenced until the York Community Stadium has been completed, safety certified and made available for use.

Reason: To ensure the satisfactory quantity, quality and accessibility of compensatory provision which secures a continuity of use and to accord with Development Plan

Policy HW3 - Built Sports Facilities.

4 Construction Management

Prior to commencement of development a Construction Management Plan shall be approved in writing by the Local Planning Authority. The approved plan shall be adhered to throughout the construction period.

The plan shall include: -

- a) Arrangements for parking of vehicles for site operatives and visitors.
- b) Means of access control, and measures to prevent construction/delivery vehicles parking/blocking the highway.
- c) Details of wheel washing facilities for the cleaning of wheels of vehicles leaving the site, including location and type.
- d) Dust - A site-specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and including a package of mitigation measures commensurate with the risk identified in the assessment.
- e) Air Quality - The air quality impacts associated with construction vehicles and non-road mobile machinery (NRMM) and the proposed mitigation measures, commensurate with the identified risk.
- f) Noise - Details on types of machinery to be used, noise mitigation and details of any monitoring and compliance with relevant standards.
- g) Vibration - Details on any activities that may result in excessive vibration, e.g. piling, and details of monitoring and mitigation to be implemented.
- h) Lighting - Details on artificial lighting and measures to minimise impact, such as restrictions in hours of operation, and the location and angling of lighting.
- i) Complaints procedure - The procedure should detail how a contact number will be advertised to the public, what will happen once a complaint had been received (i.e. investigation), any monitoring to be carried out, how they intend to update the complainant, and what will happen in the event that the complaint is not resolved. Written records of any complaints received and actions taken should be kept and details forwarded to the Local Authority every month during construction works by email to the following addresses public.protection@york.gov.uk and planning.enforcement@york.gov.uk

Reason: To ensure before development commences that construction methods will safeguard the amenities of neighbouring properties in accordance with Policy ENV2 of the City of York Publication Draft Local Plan.

5 TREE PROTECTION

Prior to the commencement of any works, including demolition within the root protection zones of trees on site, a method statement regarding protection measures for the existing trees shown to be retained on the approved drawings shall be submitted to and approved in writing by the Local Planning Authority. All works on site shall be undertaken in accordance with the approved method statement. For the avoidance of doubt this condition excludes works necessary for routine tree maintenance, pruning and crowning works.

This method statement shall include details and locations of protective fencing, and construction details where any change in surface material or installation of services is proposed within the canopy spread and likely rooting zone of a tree. No trenches, pipe runs for services or drains shall be sited within the root protection area of the tree(s) on the site which are to be retained without the prior approval in writing of the Local Planning Authority.

Reason: This condition is required pre-commencement to safeguard existing trees on the site which make a positive contribution to visual amenity and the landscaped setting of the area and biodiversity, in accordance with sections 12 and 15 of the NPPF.

6 NOISE7 Restricted hours of construction

7 Written Scheme of Investigation

a) The development shall be carried out in accordance with the Map Archaeological Practice Written Scheme of Investigation (WSI) Archaeological Evaluation by Trial Trenching report.

b) The post- investigation assessment shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (a) and the provision made for analysis, publication and dissemination of results (including publication if required) and archive deposition will be secured. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.

c) A copy of a report on the evaluation and an assessment of the impact of the proposed development on any of the archaeological remains identified in the evaluation shall be deposited with City of York Historic Environment Record to allow public dissemination of results within 6 weeks of completion or such other period as

may be agreed in writing with the Local Planning Authority.

d) Where archaeological features and deposits are identified proposals for the preservation in-situ, or for the investigation, recording and recovery of archaeological remains and the publishing of findings shall be submitted as an amendment to the original WSI. There shall be presumption in favour of preservation in-situ wherever feasible.

e) No groundworks shall take place until details in part d (above) have been approved and implemented on site and provision has been made for securing the analysis, dissemination of results and archive deposition.

Reason: In accordance with section 16 of the NPPF as the site lies within an area of archaeological interest. An investigation is required to identify the presence and significance of archaeological features and deposits and ensure that archaeological features and deposits are either recorded or, if of national importance, preserved in-situ.

8 Building Recording

The development shall be carried out in accordance with the MAP archaeological practice WSI historic building recording.

No demolition shall take place until the photographic recording as proposed in the WSI has been carried out in accordance with this report. A copy of a report shall be deposited with City of York Historic Environment Record and digital archive images with ADS to allow public dissemination of results within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

Reason: In accordance with Section 16 of NPPF and Policy D7 of the Publication Draft York Local Plan as the buildings on this site are of historic and social interest and must be recorded prior to demolition.

9 Land contamination - Site investigation

Prior to development, an investigation and risk assessment (in addition to any assessment provided with the planning application) shall be undertaken to assess the nature and extent of any land contamination. The investigation and risk assessment must be undertaken by competent persons. A written report of the findings shall be produced, submitted to and approval in writing by the Local Planning Authority prior to commencement of development. The report of the findings must include:

- (i) a survey of the extent, scale and nature of contamination (including ground gases where appropriate);
- (ii) an assessment of the potential risks to:

- human health,
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - adjoining land,
 - groundwaters and surface waters,
 - ecological systems,
 - archaeological sites and ancient monuments;
- (iii) an appraisal of remedial options, and proposal of the preferred option(s).

This shall be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

Reason: Details are required prior to commencement to ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

10 LC2 Land contamination - remediation scheme

Prior to development, a detailed remediation scheme to bring the site to a condition suitable for the intended use (by removing unacceptable risks to human health, buildings and other property and the natural and historical environment) shall be submitted to and approved in writing of the Local Planning Authority. The scheme shall include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme shall ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

11 LC3 Land contamination - remedial works

Prior to first occupation or use, the approved remediation scheme shall be carried out in accordance with its terms and a verification report that demonstrates the effectiveness of the remediation carried out must be produced and is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters,

property and ecological systems.

12 LC4 Land contamination - unexpected contamination

13 Separate systems for surface & foul drainage

The site shall be developed with separate systems of drainage for foul and surface water on and off site.

Reason: In the interest of satisfactory and sustainable drainage.

14 Site specific drainage details

Prior to development, except for the installation of any protective fencing for retained landscaping, site clearance works, remediation works and archaeological investigation works, details of the proposed means of foul and surface water drainage, including details of any balancing works and off-site works, shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details. The information shall include site-specific details of -

- a) The means by which the surface water discharge rate shall be restricted to a maximum rate of 30 litres per second.
- b) The means by which the surface water attenuation up to the 1 in 100 year event with a 30% climate change allowance shall be achieved.
- c) Full modelling calculations for the above.
- d) Future management and maintenance of the proposed drainage scheme.

The drainage details shall be shown in conjunction with the landscaping details for the site and a report provided which explains how the drainage would not compromise the landscaping details.

Reason: So that the Local Planning Authority may be satisfied with these details for the proper and sustainable drainage of the site.

15 Licence from Natural England required

Demolition works to the east stand (specifically removal of the roof and the timber clad eastern section) shall not commence unless the local planning authority has been provided with either:

- a) a licence issued by Natural England pursuant to Regulation 53 of The Conservation

of Habitats and Species Regulations 2017 (as amended) authorizing the specified activity/development to go ahead; or

b) a statement in writing from the relevant licensing body to the effect that it does not consider that the specified activity/development will require a licence.

Reason: To minimise the impacts on and to provide net gains for biodiversity, in accordance with NPPF paragraph 170, to prevent harm to a European Protected Species.

16 Bat habitat

Habitat features for bats and birds shall be provided on site in accordance with the Ecological Enhancement Plan, contained within the Quants Environmental Bat Mitigation Strategy dated 6th April 2020. The features shall be installed on the relevant houses prior to completion of construction of the relevant dwelling.

Reason: To minimise the impacts on and to provide net gains for biodiversity, in accordance with NPPF paragraph 170 and to ensure the favourable conservation status of a European Protected Species can be maintained through retaining bat roosting features on site.

17 Site-wide landscaping

The development shall not be occupied until there has been submitted to and approved in writing by the Local Planning Authority a detailed hard and soft landscaping scheme.

Notwithstanding the approved plans the scheme shall illustrate the following -

- a) The number, species, height and position of trees and shrubs.
- b) Details of all boundary treatment / means of enclosure. To include defensible space to apartments where applicable (through open boundary treatment or landscaping).
- c) Hard surfacing to roads, shared spaces, parking areas and footpaths.
- d) The detailed design of the main street, to include traffic calming measures within and around the shared surface area (such as use of surfacing materials, tree planting/landscaping and subtle variation to the alignment of the area so it appears less engineered).
- e) Details of soft landscaping incorporated within the parking courtyard behind plots 1-7.

- f) Details of means of screening the parking courtyard behind plots 12-19 (so it is not visually prominent from the public realm and is defined as a semi-private space in the interest of secure by design as car parking is poorly overlooked).
- g) Furniture to the streets and public open space (including seating areas).
- h) Means of assimilating bin and bike stores into their setting (including planting and furniture where applicable).
- i) Children's play area and trim trail within the public open space - to include setting out, surfacing and equipment.
- j) Details of the memorial area and section of retained terrace (west stand).
- k) Details of the means of enclosure and the associated planting/screening to the water supply area / pumping station (south of plot 64).

The scheme shall be accompanied by a phasing plan detailing times at which landscaping and public open space shall be completed in accordance with the approved details. The development shall be carried out in accordance with the approved details.

Any trees or plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of a similar size and species, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the site in the interests of the character and appearance of the area.

18 Public Open Space Management

Prior to first occupation of the development hereby permitted, management and maintenance plans for the public realm / open space / memorial areas (including the section of retained terrace) / play areas and landscaping shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in adherence with the approved plan(s).

Reason: In the interests of good design and the character and appearance of the area, in accordance with paragraphs 91, 92, 96 and 127.

19 Access to Public Open Space

The permissive routes, public open space and play areas hereby approved shall be kept open, free from any obstruction, in a safe condition for use by members of the public free of charge at least 364 days of the year (unless any temporary closure is reasonably required for essential maintenance).

Reason: To ensure the provision of unrestricted access for the use of members of the public, to promote health and well-being and sustainable travel in accordance with sections 8 and 9 of the NPPF.

20 Materials

Prior to construction of the building envelope of the dwellings hereby approved the following items shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details -

- a) Manufacturer's details and colour finish of all external materials.
- b) A sample panel of the brickwork to be used (which shall be erected on the site and shall illustrate the colour, texture and bonding of brickwork and the mortar treatment to be used). This panel shall be retained until a minimum of 2 square metres of wall of the approved development has been completed in accordance with the approved sample.

The houses facing Grosvenor Road (plots 1-19 and 34-7) shall have slate roofs, unless otherwise approved due to the use of roof panels for renewable energy.

Reason: In the interests of visual amenity and local distinctiveness, in accordance with paragraph 127 of the NPPF.

21 West boundary wall

Prior to first occupation of the development hereby permitted a strategy for making good the west boundary wall, including removal of blockwork, shall be submitted to and approved in writing by the Local Planning Authority, and the works carried out in accordance with the approved details.

Reason: In the interests of the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation as required under NPPF paragraph 192.

22 Cycle and bin storage

Prior to first occupation of the apartment buildings the associated cycle and bin storage shall be provided in accordance with the approved plans and thereafter

retained.

Reason: In the interests of good design, visual and residential amenity, and to promote sustainable travel in accordance with paragraphs 108 and 127 of the NPPF.

23 Connection into site to the west

Prior to first occupation of the development hereby permitted details of an access through the west boundary wall (location and design) and an associated 3 m wide footpath within the application site, connecting into the site to the west, shall be submitted to and approved by the Local Planning Authority.

The access, as detailed above, shall be carried out in accordance with the approved details, prior to occupation of the 50th dwelling, unless alternative arrangements have been approved by the Local Planning Authority.

Reason: To provide a public connection to Burton Stone Lane for pedestrians, and cyclists, at a time when this can be facilitated through the site to west. To promote sustainable travel in accordance with NPPF paragraphs 102, 108, and 110.

24 Sustainable design and construction

The dwellings hereby approved shall achieve the following measures -

a) At least a 28% reduction in Dwelling Emission Rate compared to the Target Emission Rate (calculated using Standard Assessment Procedure (SAP) methodology as per Part L1A of the Building Regulations 2013).

b) A water consumption rate of no more than 110 litres per person per day (calculated as per Part G of the Building Regulations).

Prior to first occupation of each dwelling details of the measures undertaken to secure compliance with this condition shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason: To fulfil the environmental objectives of the NPPF and supporting the transition to a low carbon future, and in accordance with policy CC2 of the Publication Draft Local Plan 2018.

25 EV charging facilities

Each dwelling with in-curtilage car parking shall incorporate a suitably rated electrical socket (to allow 'mode 3' charging) to allow charging of an electric vehicle.

Prior to first occupation a strategy for the provision of electric vehicle charging facilities to other car parking areas, at a rate of at least 5% active and 5% passive provision shall be submitted to and approved in writing. The development shall be carried out in accordance with the approved details.

Reason: To promote and facilitate the uptake of electric vehicles on the site in line with the Council's Low Emission Strategy (LES) and sections 9 and 15 of the NPPF.

NOTES

-The details required shall include an 'Electric Vehicle Recharging Point Management Plan' explaining the management, maintenance, servicing and access arrangements for each Electric Vehicle Recharging Point for a period of 10 years. It shall include details of the passive provision, details of how the charge points can be used by residents and how the bays will be managed to ensure they are only used by EVs.

-All electrical circuits/installations shall comply with the electrical requirements in force at the time of installation.

26 Travel Plan

A Travel Plan shall be developed for the site, in general accordance with the measures detailed in the updated travel plan dated April 2020 (reference Ref: 18-371-002.05).

As part of the Travel Plan the following details shall be submitted to and approved in writing by the Local Planning Authority -

- a) Preparation of a Travel Survey proforma (which shall be agreed by the City of York Council Travel Planning Officer) within 1 month of full occupation.
- b) A Travel Survey, undertaken when the site reaches 90% occupation.
- c) A Monitoring Report produced within 1 month of surveys being returned to the Travel Plan Coordinator.
- d) Agreement of Travel Plan measures and targets (which shall include measures to promote car club and explore as to whether it is viable to accommodate a car club facility/space on site) and contingency measures should targets not be met, within 1 month of surveys being returned to the Travel Plan Co-ordinator in conjunction with CYC.
- e) An annual Travel Plan review shall take place at the end of the 3rd and 5th years following the completion of initial surveys. Details of such shall be provided to the Local Planning Authority on request.

The development shall be carried out in accordance with the approved details.

NOTE: The targets for sustainable travel as detailed in the submitted plan are as follows - To reduce the number of single occupancy vehicle trips by 10%. This equates to a 2.9 percentage point reduction from 29.0% to 26.1% with a corresponding increase in sustainable modes of travel of 2.9 percentage points. The aim is to achieve this target within 5 years of full occupation.

Reason: To promote sustainable travel in accordance with section 9 of the NPPF, in particular paragraphs 108, 110 and 111.

27 Noise

The windows on the dwellings hereby permitted, shall provide a noise reduction rate (R) of a least 25 dB (rated at ≥ 25 dB R_{w+Ctr}) and standard trickle vents (rated at ≥ 33 dB $D_{n,e,w}$ (vent open)) (as recommended in section 5 of the Noise Impact Assessment by ENS).

Reason: To ensure internal noise levels are adequate and in the interests of residential amenity.

28 Plant and machinery

Prior to installation of any plant or machinery, a noise assessment (in accordance with BS4142:2014) shall be submitted to and approved in writing by the Local Planning Authority which shall evidence that externally mounted plant and equipment shall be selected and/or acoustically treated in accordance with a scheme designed so as to achieve the following noise levels -

- 30 dB LAeq (8 hour) and 45dB LAmax, no more than 10 times per night, inside bedrooms at night (23:00 - 07:00 hrs)
- 35 dB LAeq (16hour) in all other habitable rooms during the day (07:00 - 23:00 hrs)
- 50dB(A) Leq 16 hour (07:00 to 23:00) in gardens and when measured at the west boundary of the application site.

If internal noise levels are achieved with all windows shut then other means of ventilation shall be provided.

The equipment shall be installed in accordance with the approved details and reasonably maintained at all times.

Reason: In the interest of existing and future residents amenity in accordance with

NPPF paragraphs 127 and 180.

29 Removal of PD rights

Notwithstanding the provisions of The Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), there shall be no enlargements/extensions to the houses on plots 42, 43, 44, 45, 49, 62, 63, 64 that would extend beyond either the side or rear of the original dwelling house and would exceed 3 metres in height without the prior written planning permission of the Local Planning Authority.

Reason: In the interests of the amenities of the adjoining residents due to the proximity of the referenced dwellings to the site boundary the Local Planning Authority considers that it should exercise control over any future extensions which, without this condition, may have been carried out as "permitted development" under class A of the Town and Country Planning (General Permitted Development) Order 2015 (as amended).

8.0 INFORMATIVES:

Notes to Applicant

1. STATEMENT OF THE COUNCIL'S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome: gave pre-application advice, sought revised plans to address identified issues and through the use of planning conditions and a legal agreement.

2. WORKS IN THE HIGHWAY

You are advised that prior to starting on site consent will be required from the Highway Authority for the works being proposed, under the Highways Act 1980 (unless alternatively specified under the legislation or Regulations listed below). For further information please contact:

Works in the highway - Section 171 - Vehicle Crossing - Section 184 - (01904) 551550
- streetworks@york.gov.uk

3. LEGAL AGREEMENT

Your attention is drawn to the existence of a legal obligation under Section 106 of the Town and Country Planning Act 1990 relating to this development

Contact details:

Case Officer: Jonathan Kenyon

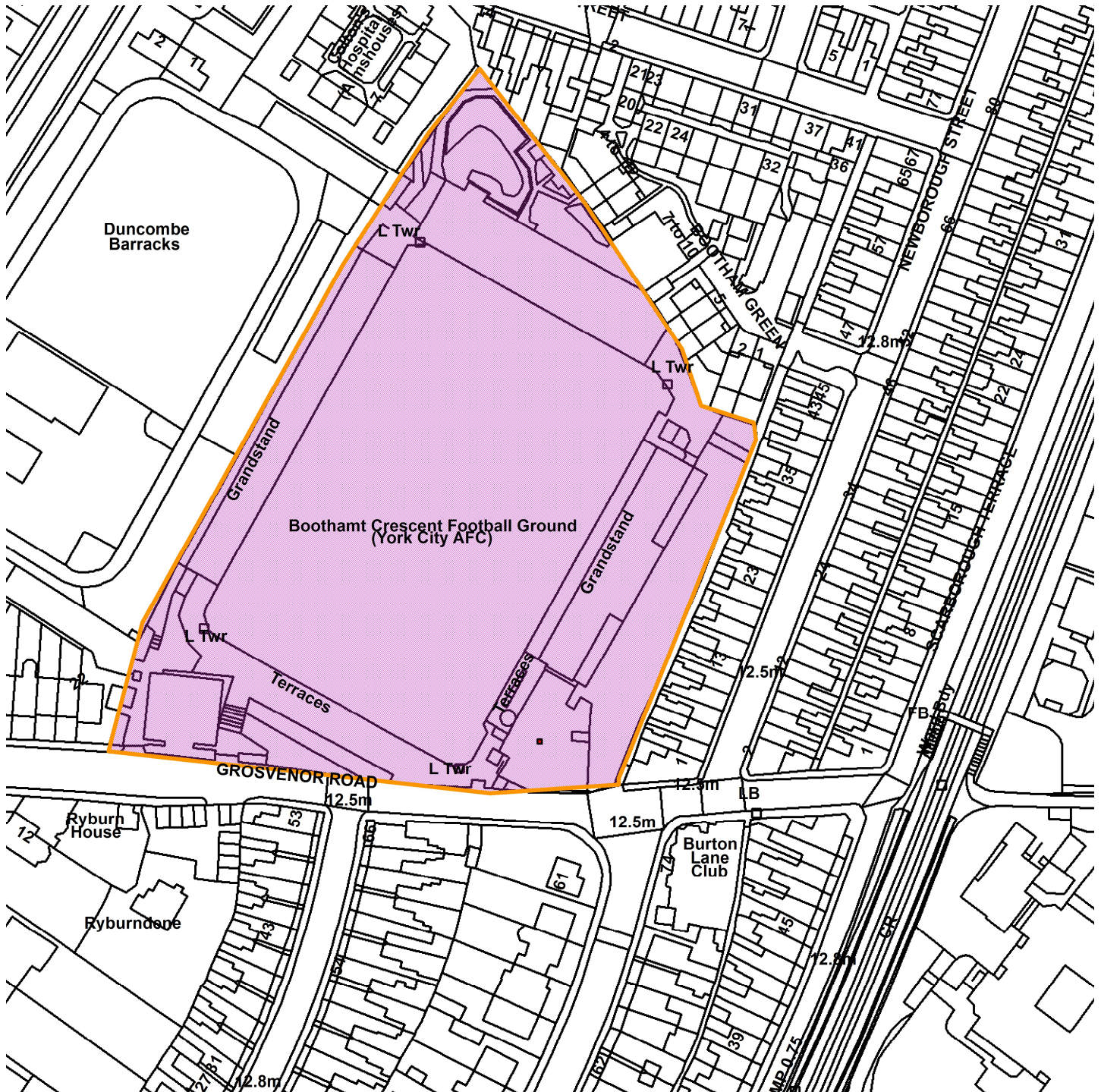
Tel No: 01904 551323

York City Football Club, Bootham Crescent, York

19/00246/FULM



GIS by ESRI (UK)



Scale : 1:1442

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plan
Date	24 July 2020
SLA Number	

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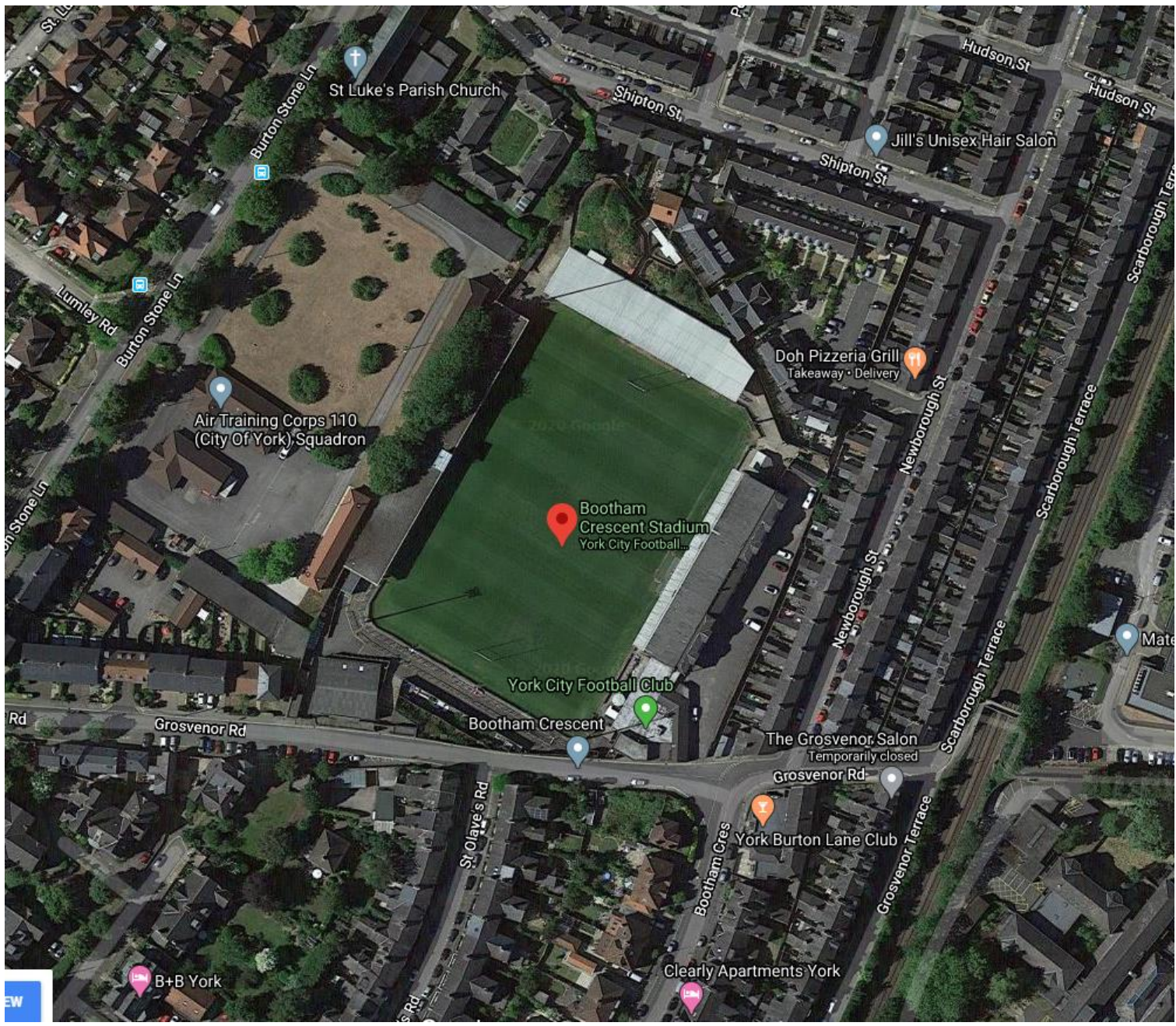


Planning Committee - Thursday 13 August 2020

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York City Football Club

Bootham Crescent



City of York Council Planning Committee -Thursday 13 August 2020

Grosvenor Road

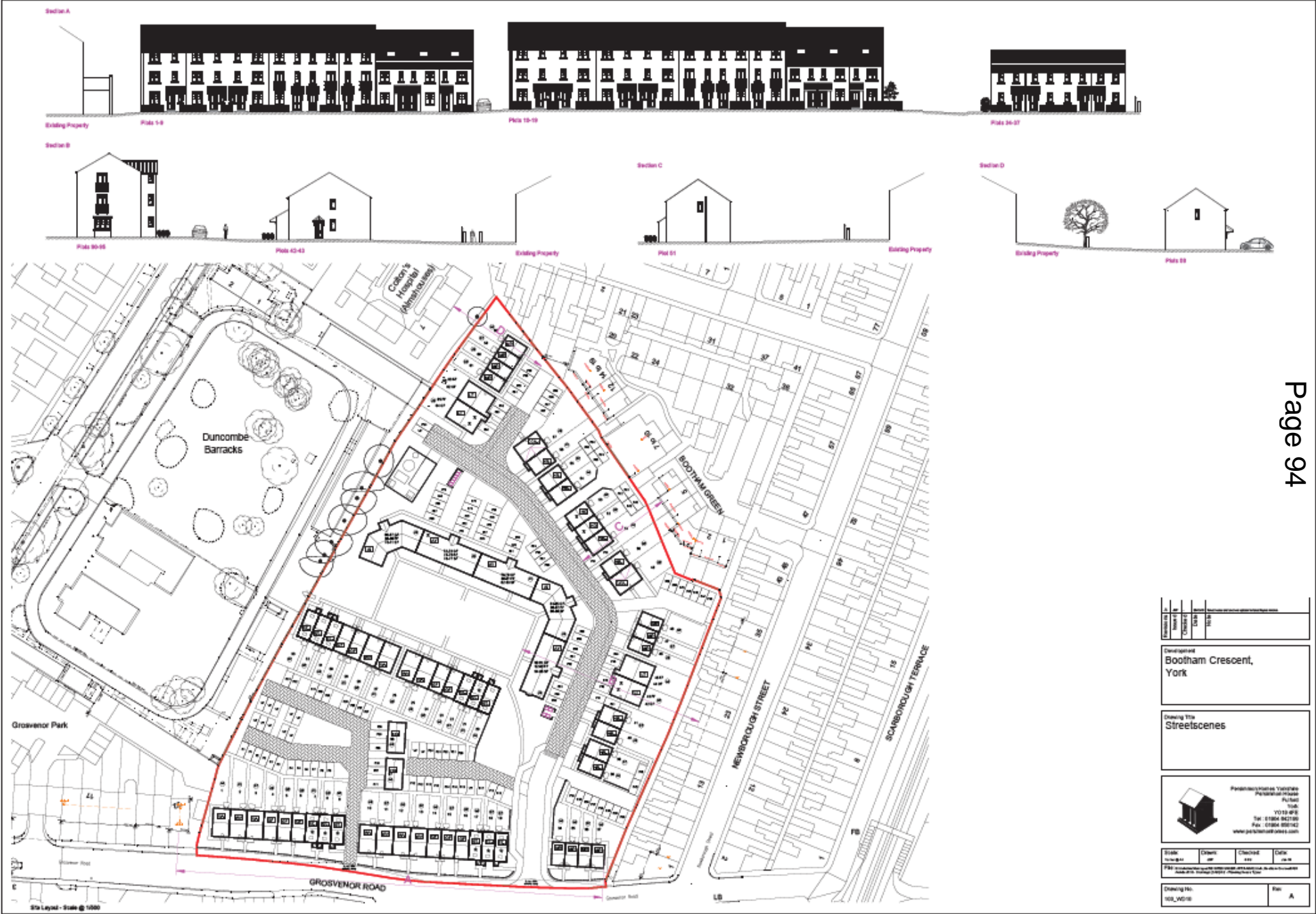



City of York Council Planning Committee -Thursday 13 August 2020

Trees on
boundary from
Burton Stone
Lane



Streetscenes Including Grosvenor Road Frontage



Author	Drawn	Checked	Date
Checked	Scale	Client	
Development Bootham Crescent, York			
Drawing Title Streetscenes			
 Pennington Urban Planning and Architecture York Tel: 01904 842100 Fax: 01904 850142 www.pennington.co.uk			
Scale	Client	Checked	Date
1:500	Y10	Y10	10/10/10
Drawing No. 101_WD10			
			Rev A

Aiden Apartment Type

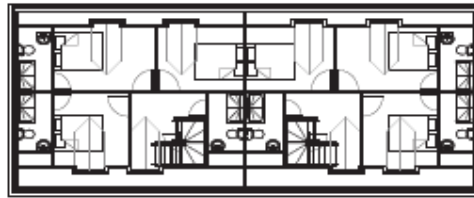


PERSIMMON
HOMES



646sqft	
Aidan	180413
AI-WD10	1000A1, 2000A2
Plans & Elevations (Planning)	
	Rev-

Aycliffe 3.5 storey Apartment Type



Third Floor



Second Floor



First Floor



Ground Floor



Front Elevation

Side Elevation



Rear Elevation

Side Elevation

PERSIMMON
HOMES



803 / 1257 sqft	
Aycliffe (3.5 Duplex)	020503
AY-3.5-WD10	1000@+1, 2000@+2
3.5 Storey Aycliffe (with Duplex)	

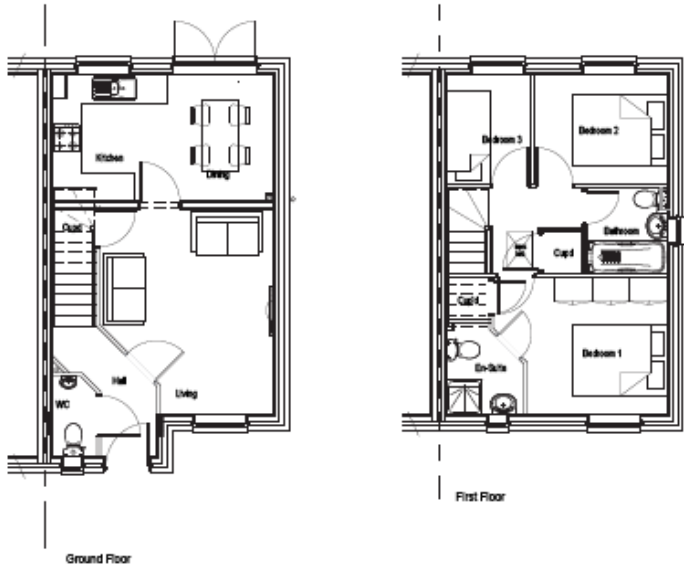
Flat Over Garage House Type

The architectural drawings for the Flat Over Garage House Type include the following components:

- Elevations:** Front Elevation, Rear Elevation, and two Side Elevations showing the exterior facade and roof profile.
- Sections:** A cross-section showing the internal structure, including the garage level and the living area above.
- Floor Plans:** Ground Floor (showing three garage units and a central staircase) and First Floor (showing a living area, kitchen, and bedrooms).
- Detail Bandings:**
 - Typical Eaves Brickwork Detail Banding:**
 - Brickwork Elevation: Hatching indicates projecting brickwork (Max. 15mm).
 - Brickwork Section: Shows the profile of the brickwork projection (Max. 15mm).
 - Typical Brickwork Detail Banding:**
 - Brickwork Elevation: Hatching indicates projecting brickwork (Max. 15mm).
 - Brickwork Section: Shows the profile of the brickwork projection (Max. 15mm).

PERSIMMON HOMES	
486sqft	
FOG 1 Bed	181718
FOG1_WD10	906A1, 1006A2
Plans & Elevations (Planning)	
	Re-

Barton House Type



	
761sqft	
Barton	M218
HB-WD10	50@A1, 100@A5
Plans & Elevations (Planning)	
	Rev -

Carleton House Type

The architectural drawings for the Carleton House Type are organized as follows:

- Elevations:**
 - Front Elevation:** Shows a two-story facade with a central entrance door, a window above it, and a larger window on the ground floor to the left. The upper story has a window with horizontal cladding above it.
 - Rear Elevation:** Shows a two-story facade with a central double door, a window to the right, and a window above the door. The upper story has a window with horizontal cladding above it.
 - Side Elevation (Left):** Shows a gabled roofline with a single window on the upper floor.
 - Side Elevation (Right):** Shows a gabled roofline with a small window on the ground floor.
- Floor Plans:**
 - Ground Floor Plan:** Labeled with 'Living Area', 'Kitchen/Dining', and 'Cupb'. It shows a central staircase and a front entrance.
 - First Floor Plan:** Labeled with 'Bedroom 3', 'Bedroom 2', 'Bathroom', and 'Yard'. It shows a central staircase and a rear entrance.
 - Second Floor:** Labeled with 'Bedroom 1' and 'Cupb'. It shows a staircase and a rear entrance.

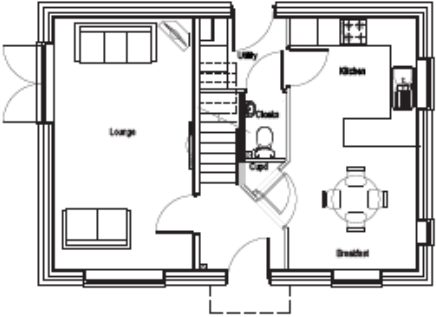
Plots 20-22 & 27-29 only

PERSIMMON
HOMES

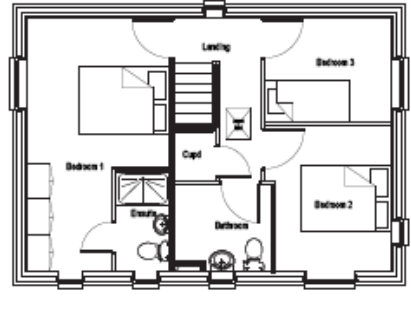


980sqft	
Carleton	22818
ST-WD10	500@A1 100@A3
Plans & Elevations (Planning)	
	Rev-

Lockwood Corner House Type




Ground Floor Plan.



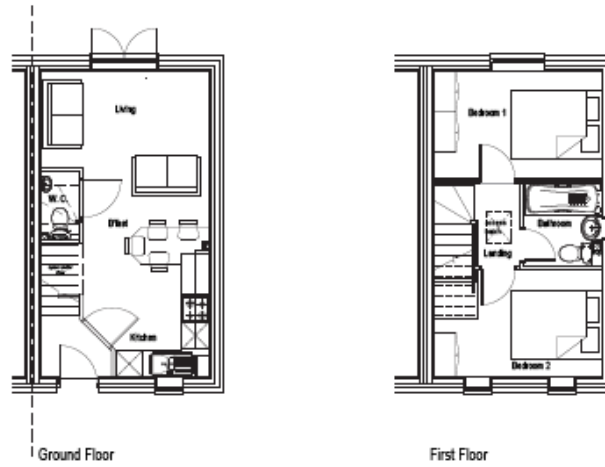
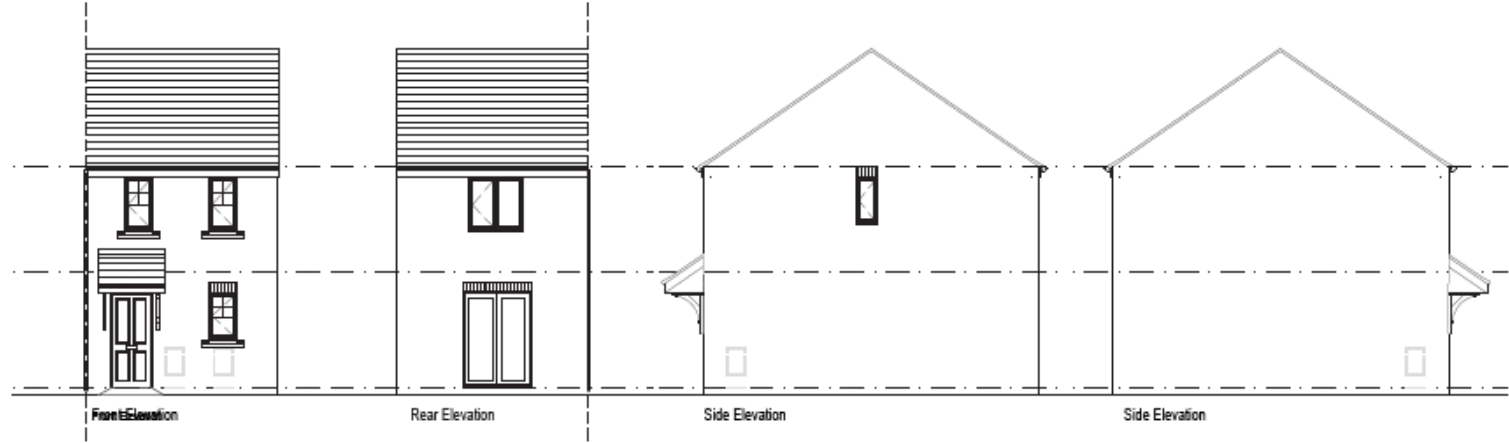
First Floor Plan.

PERSIMMON HOMES



809sqft	
Lockwood Corner	10/20
CCA-WD10	1000A1, 2000A2
Plans & Elevations (Planning)	

Morden House Type



Plots 46-48 & 58-61 only

PERSIMMON HOMES



553 sqft	
Morden	141218
MR-WD10	50@A1, 100@A3
Plans & Elevations (Planning)	
	Rev P

Ullswater House Type



Plots 23, 24, 30 & 31 only

PERSIMMON
HOMES



1097sqft	
Ullswater	141218
GF-WD10	55@A1, 100@A5
Plans and Elevations (Planning)	
	Rev -

Ullswater House Type
(Grosvenor Road frontage)



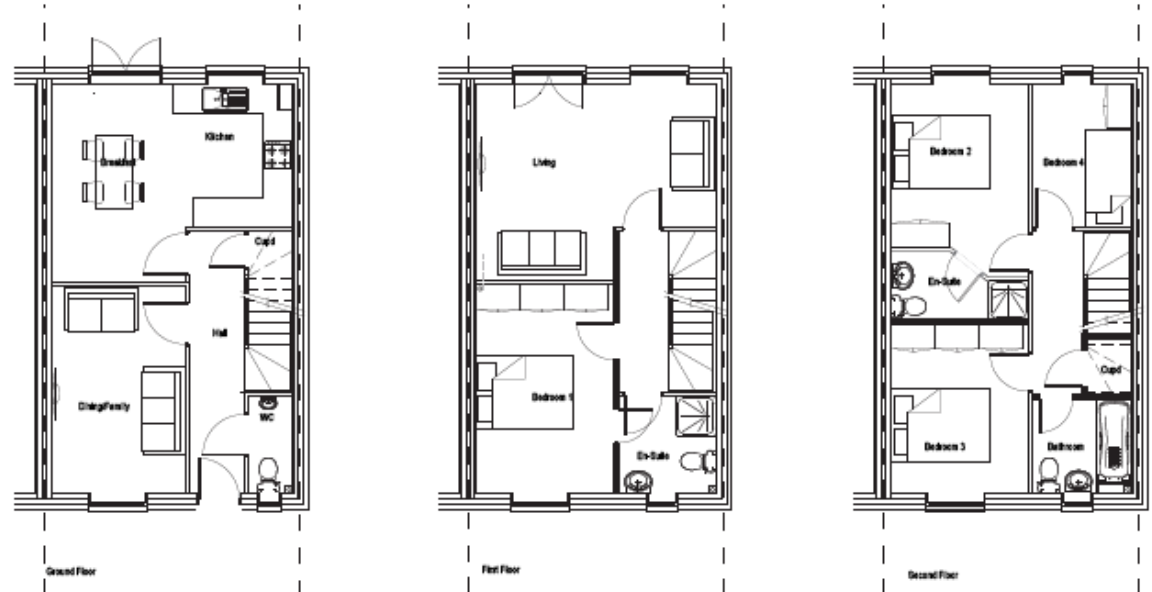
Plots 4 & 10-14 only

PERSIMMON HOMES

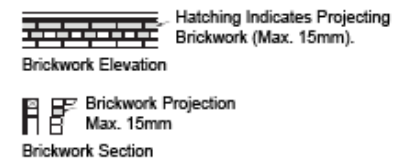


1097sqft	
Ullswater	141218
GF-WD10-2	50@A1, 100@A3
Plans and Elevations (Planning)	
	Rev -

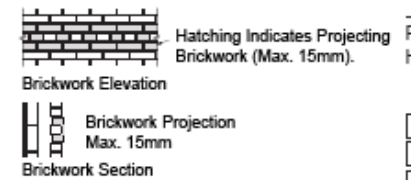
Wolvesley House
Type (Grosvenor
Road frontage)



Typical Eaves Brickwork Detail Banding



Typical Brickwork Detail Banding



Plots 1-3, 8, 9, 15 & 16 only

PERSIMMON HOMES



1339sqft	
Wolvesley	141219
WV-WD10-2	50@A1, 100@A3
Plans & Elevations (Planning)	
	Rev G

COMMITTEE REPORT

Date: 13 August 2020 **Ward:** Guildhall
Team: East Area **Parish:** Guildhall Planning Panel

Reference: 19/02563/FULM
Application at: 23 Piccadilly York YO1 9PG
For: Erection of no.132 bed hotel with bar/ restaurant, after demolition of existing office building
By: Mr Gareth Jackson
Application Type: Major Full Application
Target Date: 17 August 2020
Recommendation: Approve

1.0 PROPOSAL**APPLICATION SITE**

1.1 The existing office building on-site is dated 1939. It has a stone plinth (which varies in height to suit local ground levels) with 3 floors and mansard roof above. The building is not listed and nor is it identified as a building of merit in the Central Historic Core Conservation Area Appraisal. The site is in the Central Historic Core Conservation Area; the Piccadilly character area. It is an urban block surrounded by streets between Walmgate and Piccadilly, with a varied context.

1.2 St Denys's Church and its grounds are to the north-east. The church is listed at Grade I. Along St Denys' Road are 2-storey domestic buildings and a 7-storey mid-20th century office block, sat behind car parking, recently converted to apartments. This building; formerly United House is identified as a detractor in the conservation area appraisal. To the opposite side along Dennis Street is the rear access to the temporary Spark commercial complex, recently completed housing at Nelson's Court which is 3.5 storey and more historic domestic sized properties on Dennis Street. Opposite the site on Piccadilly a hotel building is currently under construction. Next door to that site is Ryedale House, another mid 20th century office block, that is currently being modernised and converted into residential.

PROPOSALS

1.3 The application is for a 132 bedroom hotel, reduced from 146 since the original submission. The proposed building is part 6, part 4 storey. The Piccadilly side of the building would be sheer 5-storey with the top floor recessed, with an angled façade in decorative brick. The front of house area, with reception and restaurant,

would front onto Piccadilly giving the building an active frontage. The pavement will be widened here and the public realm improved, as part of the scheme, following aspirations within the Castle Gateway masterplan. The rear section of the site steps down to 4-storey (lower than the existing building) to respect the scale and setting and neighbouring Nelson's Yard housing and the Grade I listed St Denys Church.

1.4 Servicing will be from St Denys Road. The road is one way but has 2 lanes connecting into Piccadilly. A drop off is also allowed for on Piccadilly by the main entrance. There is no car parking, as per the existing offices.

BACKGROUND / RELEVANT SITE HISTORY

1.5 The existing building was subject to an application for change of use to residential (prior approval under permitted development rights) to which there were no objections in 2017 - 17/02624/ORC.

2.0 POLICY CONTEXT

2.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that determinations be made in accordance with the development plan unless material considerations indicate otherwise.

2.2 The National Planning Policy Framework ('NPPF') is a material consideration in the determination of this planning application. Key policies / sections of the NPPF are as follows –

- 6 Building a strong competitive economy
- 7 Ensuring the vitality of town centres
- 12 Achieving well-designed places
- 14 Meeting the challenge of climate change, flooding and coastal change
- 16 Conserving and enhancing the historic environment

2.3 The Publication Draft City of York Local Plan 2018 ('2018 DLP') was submitted for examination on 25 May 2018. In accordance with paragraph 48 of the NPPF its policies can be afforded weight according to:

- The stage of preparation of the emerging plan (the more advanced the preparation, the greater the weight that may be given);
- The extent to which there are unresolved objections to relevant policies (the less significant the unresolved objections, the greater the weight that may be given); and
- The degree of consistency of the relevant policies in the emerging plan to the policies in the previous NPPF published in March 2012.

2.4 Key relevant Publication Draft Local Plan 2018 Policies are as follows -

Application Reference Number: 19/02563/FULM

Item No: 4c

SS3 York City Centre
SS5 Castle Gateway
EC1 Provision of Employment Land
EC4 Tourism
D1 Placemaking
D4 Conservation Areas
D5 Listed Buildings
D6 Archaeology
CC1 Renewable and Low Carbon Energy Generation and Storage
CC2 Sustainable Design and Construction of New Development
ENV4 Flood Risk
ENV5 Sustainable Drainage

3.0 CONSULTATIONS

INTERNAL

DESIGN, CONSERVATION AND SUSTAINABLE DEVELOPMENT

CITY ARCHAEOLOGIST

3.1 During construction of the extant building at 23 Piccadilly in the 1930s two rows of columns were recorded. These were interpreted at the time as a Roman wharf.

3.2 As part of this application several phases of evaluation have now taken place including a three point borehole survey, a ground penetrating radar (GPR) survey and two 2m x 2m evaluation trenches within the basement. Trench 1 at the Piccadilly frontage revealed a possible foundation and dump deposit interpreted as Roman at the base of the trench c. 7.05m AOD.

3.3 The archaeological evaluation has suggested that potential Roman archaeological deposits and features exist c. 1.2m below the existing basement floor at 6.85m AOD. These will likely be affected by the foundation design and potentially the lift pit.

3.4 Based on the evaluation to date officers are content works can progress subject to conditions for a watching brief on groundworks and an approved foundation design to preserve 95% archaeology.

DESIGN CITY ARCHITECT

3.5 Officers consider the scheme has less than substantial harm to the significance of the panoramic key view from Clifford's Tower (one of the key views in the Central Historic Core Conservation Area Appraisal). The impact is deemed to be very minor though in terms of the setting of St Denys's Church.

3.6 The proposed building competes with the height of the tower of St Denys' Church in views to a minor degree, because whilst it does not obscure the tower, it sits immediately to the side of it.

3.7 The additional impact of this proposal is minor compared to the impact of 46-50 Piccadilly. There is cumulative impact to consider when adding both, although there is still less than substantial harm to the setting of the church. The harm is the interrelationship with Clifford's Tower "key view 16". The exceptional quality of this view is acknowledged, when the view is taken in the round, as its significance is linked to it being a 360 degree view, rather than a static view of a particular corridor. Therefore I only consider very minor impact on the significance of this view resulting out of the proposal, and so the harm to the church is, to me, at the lower end of less than substantial despite the exceptional significance of key view 16.

3.8 With regards the existing building whilst neutrally identified in the conservation area appraisal, would undoubtedly be a non-designated heritage asset if it had the chance to undo insensitive changes through renovation- like restoring windows to the original design. The value of this building to the setting of the church is how it appropriately contextualises the church to the south west. Officers view is the proposed building does not harm the setting of St Denys's as it is experienced from local streets.

HIGHWAY NETWORK MANAGEMENT

Impact on the network

3.9 Officers anticipate that a development of this scale will have a slight negative impact on the operation of Piccadilly due to additional trips by taxi/coach trips. No parking is proposed as part of the scheme. This is an acceptable approach in this location and for this development purpose. As part of the Travel Plan, the applicant should propose a strategy to encourage guests to travel to and from the site by sustainable means. The applicant should also provide a strategy to direct those guests accessing the site by car to nearby car parks.

3.10 A hotel/bar/restaurant is likely to generate a significantly higher number of servicing trips compared to the existing office use. The proposed service area on St Denys is acceptable to highways. The street is one way only and wide enough to enable traffic to pass by a loading vehicle. Double yellow lines are in place in this location so loading would need to take no more than 10 minutes. Any amendments to the TRO (as proposed in the supporting Transport Statement) will need to be funded by the applicant.

Construction management

3.11 Conditions requested - dilapidation survey, keeping of highway clean during works and management of vehicular movements and contractor parking.

Temporary parking restrictions might be required and will need to be requested in advance from the highway authority.

3.12 The secondary pedestrian access point on Dennis Street still seems to show a door that opens outwards onto the footway. Doors need to be set so they would not encroach onto the footway area when open.

Cycle parking

3.13 Steps are proposed internally from the back of the house entrance on St Denys Road. Preferably there would be a ramp for loading/servicing and to gain access to the cycle store. The access door needs to be wider than the standard 0.9m width and automated if possible. 14 spaces are provided: 8 stacked and 6 Sheffield Stand – details should be provided through condition.

Public Realm

3.14 The layout of Piccadilly would be agreed with the highway authority at a later stage as part of the Castle Gateway / Piccadilly Masterplan, through S278 highways agreement process. This includes the planters and trees shown in the D&A statement as their location / feasibility depends on utilities, visibility.

PUBLIC PROTECTION

Noise

3.15 Officers ask for approval of a scheme to demonstrate noise breakout from the restaurant/bar area will not have an undue impact on noise levels required within bedrooms. It is asked that noise levels in the bedrooms achieve BS4142: 2014 compliance.

Deliveries

3.16 Ask for delivery times to be restricted.

Construction management

3.17 Request measures to control noise/vibration, dust and light pollution and restricted hours of working.

Odour

3.18 Restaurant – measures to deal with cooking odours requested.

Land contamination

3.19 Require for remediation strategy to be approved and implementation

EXTERNAL

CONSERVATION AREAS ADVISORY PANEL

3.20 The panel objected to the original scheme

- The current building whilst an undistinguished 1930s construction is an example of its time, now somewhat rare in the city centre, and gives some dignity to the street and therefore the Conservation Area. The Panel questioned the need to demolish the building and considered a use could be found which would ensure its retention.
- It was felt strongly the replacement building with its false gables had no architectural merit, would harm the setting of the adjacent Grade I listed St Denys Church and could be regarded as a detractor to the Conservation Area.

HISTORIC ENGLAND

3.21 Historic England have concerns over the application. Their preference would be that the scale and height of the development be reduced to avoid any impact on the views of St Denys' Church from Clifford's Tower. The scheme is deemed to have less than substantial harm on the setting of St Denys Church.

3.22 In the revised scheme the elevation treatment, including the form and size of the window openings, and the use of brick detailing, are all improvements in terms of design quality. There would be a reduction in the size of the building closest to St Denys' Church which would reduce its impact on the immediate setting of the church to some degree, however the proposed new building would still be a full storey higher than the existing building fronting Piccadilly which means that key views and the wider context of the church would still be significantly affected.

3.23 The tower and the north aisle of St Denys' Church can be appreciated from Clifford's Tower. The ability to appreciate a number of churches and spires from the top of the Clifford's Tower makes a strong contribution to illustrating the relationship between the River Foss, the Castle defences and the area of the medieval settlement. This creates a strong sense of place with a great sense of time depth and character.

3.24 HE advise that the view of the tower of the St Denys' Church from Clifford's Tower should be maintained. The 6th floor block on the front section of the proposed building would compete with the height of the tower of St Denys' Church when viewed from Clifford's Tower and that the view of the church would still be partly obscured.

PLANNING PANEL

3.25 Object as follows—

- The existing building is of architectural interest and thus deserves preserving.
- The building height appears to be out of keeping with the rest of the vernacular architecture and particularly in relation to St Denys.
- The building profile at the front and the basement placing of the restaurant presents an un-friendly aspect to the street frontage.
- Concerned about the potential disturbance of the foundations of St Denys Church
- The services are going to obstruct the pavement and the road on St Denys Road, which is a busy bus route.
- Concerned the local infrastructure will not be able to cope with yet another hotel sending a large amount of sewage into the drains.

POLICE

3.26 With regards secure by design officers recommended the following –

- Access points should be well lit
- Controlled lift and stairwell access is recommended. This will increase the security of the hotel as only guests
- Public entrances and exits into and out of the building should be covered by CCTV and in particular the entrance foyer/reception area.
- Secure cycle parking should be provided for the staff. The stands must facilitate the locking of both wheels and the frame and should be located so that it can be supervised by hotel staff as cycle crime is an issue in the area.

YORK CIVIC TRUST

3.27 Object due to the environmental impact of the loss of the building, the scale and design of the proposed building and intensification of use of the surrounding area and possible implications for operation of the church.

Unjustified environmental cost

- Further to the latter point, considering the Climate Emergency declared by York City Council in March 2019, the Trust feels that it is irresponsible to condone the loss of a building that has not come to the end of its useful life.

Visual impact

- The proposal is gross over-development, five storeys being too tall for such a sensitive location and would result in partially blocking Key View 6 (as identified in the York Central Historic Core Conservation Area Appraisal) from Clifford's Tower to St Denys Church.

3.28 Rather than having the effect of reducing massing, the glazed fifth floor is thought to be a detractor, which will reflect the sun by day and become a beacon of light at night.

Access

- Impeded access between St Denys Church and the Church Hall

The hall contains the church's only toilet and is a much-used community asset. The change of use on the site from offices to a hotel will result in significantly different access requirements and will necessitate frequent deliveries and taxi pick up/drop off.

4.0 REPRESENTATIONS

4.1 There was a second round of consultation on the revised scheme in July 2020. Overall objections, from 25 contributors, have been received. Comments as follows

-

IMPACT ON ST DENYS CHURCH AND ITS GROUNDS AND THE CHURCH HALL

4.2 The church stands very close to the potential building site of the proposed new hotel. Extensive structural works have recently (2016-8) taken place at the church, at a cost of over £300,000, whose principal purpose was to stabilise and restore the previously critically endangered north aisle. This essentially dates from c.1340 and contains outstandingly important mediaeval stained glass. The aisle proved to have only very sketchy foundations, especially to the north aisle west wall (the wall nearest the proposed hotel building site). There is concern that the impact of the proposed demolition and building works and the heavy traffic likely to be involved, may endanger and further destabilise not only the church's north aisle but also other parts of the church.

4.3 Concern building works will prevent or impede use of the church hall. The church hall serves as a community facility open to use by all and at a low cost.

4.4 Concern over the possible loss of mature trees within the church grounds as a consequence of demolition.

ASPIRATIONS FOR CASTLE PICCADILLY / OBJECTION TO HOTEL IN PRINCIPLE

4.5 The proposed use would be in conflict with the aspirations for Castle Piccadilly. The wide-ranging consultation for Castle Gateway found a desire for small-scale businesses creating interest and diversity. The adjacent Walmgate area provides an excellent example of this process in action. Instead proposed for this site is a

large corporate hotel; there are already two similar proposals for this type of development along Piccadilly. The concentration of hotels in the site has a detrimental impact due to their transient nature and disturbance caused by some guests.

4.6 There is some evidence that there is now an excess of supply over demand for hotels, which in turn results inevitably in the reduction of prices, so that what was intended as so called mid-range becomes cheaper, lower range. Therefore question the desirability and viability of yet more corporate style hotel accommodation.

4.7 The site is currently in-use which demonstrates need for the existing use. There is a preference for retention of offices or conversion to residential – both are needed in the city, rather than another hotel.

4.8 The function and design of the surrounding streets should be re-considered, putting pedestrians first in a similar way to around Clifford's Tower; to improve the public realm and be a more attractive part of the pedestrian/cycle network (including the aspirations for a new bridge over the Foss).

UNNECESSARY DEMOLITION / UNSUSTAINABLE APPROACH

4.9 The existing building does not detract from the area and should be retained. It arguably makes a positive impact being of historic and architectural interest; it is a good example of inter-war British Architecture, of good quality materials and detailing and of appropriate scale. It represents a style of 1930s-50s 'official' architecture which is now becoming unusual nationally, and which is very rare in York. Buildings of this kind are now beginning to be listed Grade II in other parts of the country.

4.10 The existing building makes efficient use of the site, being 4-storey and utilising the majority of the plot. A new use should be found for the building rather than demolition; an unsustainable option (therefore consequently against the principles of the NPPF), the process of which will cause considerable local disruption to surrounding occupants and the transport network.

4.11 Should this building be demolished given the amount of change and recent demolition / new build that would occur in Piccadilly, its inclusion within Conservation Area would no longer be justified.

VISUAL IMPACT OF THE PROPOSED BUILDING

4.12 The building due to its bulk and height is inappropriate and out of character with the traditional scale of buildings within the city. There would be harm to the setting of the surrounding buildings.

4.13 Its harsh, brutal like appearance will detract from the setting compared to the light brick and design of the existing building.

NOISE / DISTURBANCE / POLLUTION

4.14 Concern over the disturbance as a consequence of the activities associated with the comings and goings of a hotel – staff, guests and servicing which will be constant / Putting out of waste, such as glass at night / Possible noise from plant and machinery / Disruption during building works.

IMPACT ON THE HIGHWAY

4.15 Another hotel will bring in more traffic. Generally around 80% of guests arrive by car and hotel operators have arrangements with local car parks. No car parking only means guests arrive expecting to park but can't. The two nearest car parks (Peel Street and on Piccadilly) are already seriously over-subscribed, and illegal / indiscriminate on-street parking in the surrounding area is already a serious problem. The hotel can only exacerbate this problem and create safety issues.

Servicing

4.16 Originally servicing appeared to be from Dennis Street. This was objected to as such activity would block the access to residential development at Nelson's Yard. The revised submission contained an updated transport assessment and confirmed the intention was for servicing from St Denys. This led to further objections.

- The proposed service bay would create traffic delays and increased pollution.
- Loss of car parking.
- St Denys is primarily residential and the servicing associated with a hotel will cause undue noise disturbance. It is asked that deliveries be restricted to daytime hours and that taxi drop off be from Piccadilly.

IMPACT ON AMENITY DUE TO SCALE OF BUILDING

4.17 The building proposed is still a storey taller than the existing where facing Piccadilly; there will be overlooking / loss of privacy / loss of sunlight to surrounding houses, on Nelson's Yard and St Denys. Loss of views of Clifford's Tower would devalue surrounding properties.

ARCHAEOLOGY

4.18 Roman activity in this area and therefore the site should be subject to adequate investigation / preservation of remains.

FLOOD RISK

4.19 Inadequate commentary and details of how the building would be flood resilient, in particular to basements areas. It is also noted that no other development have been permitted to have bedrooms at levels susceptible to flooding.

5.0 APPRAISAL

5.1 The key issues are as follows –

- Principle of the proposed use
- Design
- Impact on designated heritage assets (listed buildings / conservation area / archaeology)
- Impact on residential amenity
- Highway network management and safety
- Drainage / Flood Risk
- Public Protection
- Sustainable design and construction
- Ecology

PRINCIPLE OF THE PROPOSED USE

5.2 Both the existing and proposed uses are ‘town centre’ uses in NPPF terms and are appropriate in this part of the city centre according to the NPPF and strategic policy SS3 of the 2018 DLP, which sets out the approach for the city centre.

5.3 The economic objectives of the NPPF are to build a competitive economy and support growth. Paragraph 80 states “decisions should help create the conditions in which businesses can invest, expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development”. Fundamentally in principle the application accords with the economic objectives of the NPPF.

5.4 Within the 2018 DLP there are aspirations to avoid the loss of office space, if it is viable and demonstrably needed to meet identified needs (policy EC2). Whilst policy EC4 Tourism seeks to maintain and improve the choice of visitor accommodation. The background text to EC4 states “A key aim of the York Economic Strategy (2016) is to continue to creatively develop York’s tourism and culture offer and to raise the city’s profile as a quality visitor destination”. Local policies can be applied with limited weight only given that the plan is not adopted.

5.5 The existing building provides low cost / low quality office space, which would need significant investment to accommodate grade A quality space. Policy EC2

which relates to the loss of employment land stresses the need to retain grade A / grade 1 spaces in 'high access locations'. The grade of office space provided affects the value of the current building to the landowner and there is risk associated with any re-provision / upgrade which would be speculative.

5.6 The tourist industry is a key component of the York economy and is experiencing growth. In 2019 York was in the UK top ten for regional markets (considering revenue per room) and occupancy rates of over 80%. There is evident need for further hotel accommodation and the 2018 DLP promotes growth of the sector.

5.7 The tourist sector is experiencing growth and the re-build proposed allows more efficient use of the site, increasing useable floorspace from 2,908 sq m to 4,615 sq m. As such the proposed re-use of the site is consistent with the economic policy and policies for making effective use of land within the NPPF.

DESIGN

5.8 NPPF paragraph 127 states that planning decisions should ensure that developments:

- will function well and add to the overall quality of the area;
- are visually attractive as a result of good architecture, layout and landscaping;
- are sympathetic to local character and history, while not preventing or discouraging appropriate innovation or change (such as increased densities);
- establish or maintain a strong sense of place;
- optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space); and
- create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

5.9 Local Plan policy D1 Place-making contains local design policy and covers –

- Urban structure and grain
- Density and massing
- Streets and spaces
- Building heights and views
- Character and design standards

5.10 Policy SS5 for the Castle Gateway with regards to Piccadilly has aspirations that regeneration schemes provide active frontages and contribute to public realm improvements (reducing the size of the vehicular carriageway on Piccadilly and improve the size and quality of the pedestrian foot streets, including tree planting).

5.11 Since the application was submitted there have been significant amendments to the scheme. The number of bedrooms has reduced to 132, the building scale has been moderated to reflect the urban grain, and the design has been given further thought; materials chosen to respect local vernacular and detailing refined to give increased visual interest. The servicing is from St Denys Road (as currently) and the scheme contains provision for public realm improvements consistent with the most recent Council plans for Piccadilly. The changes are illustrated in the Design and Access addendum.

Scale & massing

5.12 The building proposed is taller on its Piccadilly side, where it would be 6-storey. The top floor would be higher than the existing building; it would be setback from the edge of the main façade to reduce its prominence. The rear section of the building steps down to 4-storey - this section is lower than the existing building. Both side elevations have elements, most evident on the Dennis Street side, where the building line is cranked on the upper floors; to reduce the prominence of the building and reveal St Denys's Church in views.

5.13 The taller scale on the Piccadilly side of the site is appropriate; the existing building is of lesser scale and height than surrounding buildings it is viewed in context with along Piccadilly. Only the top floor of the proposed building exceeds the height of the existing building and this level is designed so it will be subtle in views from the street. Two of the neighbouring buildings; Ryedale House and Piccadilly Residence (previously United House) are both some 2-storey taller, the two developments to the south are a mix of 5 and 6 storey and the proposed building scale is comparable to the approved hotel (under construction) at 46-50 Piccadilly directly opposite.

Public realm / frontage

5.14 The hotel would have a front of house area – reception and restaurant at ground level fronting Piccadilly and involves public realm improvements. The footpath would be widened and there is capacity to accommodate cycle stands, seating and planting. These improvements would be secured by condition. The existing building has a substantial plinth at ground level and has little interaction with the street. The scheme in this respect conforms with the Local Plan aspirations for Piccadilly.

Materials

5.15 The proposed building is now predominantly (red) brick and no longer contains metal cladding as a secondary material to the façade and roof. The design contains brick detailing that gives order, visual interest and decoration.

5.16 The building will be contemporary, attractive and respectful of its setting. Officers are content that the scheme complies with local policy D1 – place-making in

respect of ensuring proposals are not a pale imitation of past architectural styles, the use of appropriate materials and the scheme would be a good example of contemporary urban design and place making.

IMPACT ON DESIGNATED HERITAGE ASSETS (LISTED BUILDINGS / CONSERVATION AREA / ARCHAEOLOGY)

5.17 The Council has a statutory duty (under section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990) to consider the desirability of preserving or enhancing the character and appearance of designated conservation areas. Section 66 of the 1990 Act requires that in considering whether to grant planning permission for development, which affects a listed building or its setting, the local planning authority shall pay special regard to the desirability of preserving the building or its setting or exercise of any features of special architectural or historic interest which it possesses. The Courts have held that when a local planning authority finds that a proposed development would harm a heritage asset the authority must give considerable importance and weight to the desirability of avoiding such harm to give effect to its statutory duties under sections 66 and 72 of the 1990 Act. The finding of harm to a heritage asset gives rise to a strong presumption against planning permission being granted.

5.18 The approach to determining planning applications, in terms of assessment on Heritage Assets, is set out in section 16 of the NPPF. The starting point is to understand the significance of the Heritage Assets affected. In considering impact, where a development proposal will lead to “less than substantial harm” to the significance of a designated heritage asset, this harm must be weighed against the public benefits of the proposal. Public benefits can derive from either of the social, environmental or economic objectives of the NPPF.

5.19 The Heritage Assets that require assessment are as follows –

- St Denys’s Church - Grade I listed
- The Central Historic Core Conservation Area
- City Centre Area of Archaeological Importance

Assessment of impact

Setting of St Denys Church

5.20 The church dates from the 14th and 15th Century. The listing description references in detail its historic architectural features, both interior and exterior and its stained glass, which dates from the 13th, 14th, and 15th centuries. It sits tightly within its grounds, which are elevated in comparison to the surrounding streets. It is primarily experienced from Walmgate with the application site as a backdrop. The

north aisle and upper section of its tower are currently evident in views from Clifford's Tower.

5.21 The north aisle will be obscured in views from Clifford's Tower when the hotel on Piccadilly is complete. Historic England have raised concerns that the proposed top floor will obscure views of the church tower from Clifford's Tower. In the Central Historic Core Conservation Area Appraisal the 360 degree view from Clifford's Tower is identified as a key view (number 16). The appraisal states that "the towers and spires of the city's churches form another significant group in this view, highlighting in their density the paramount role of the Church in medieval life. The character and materials of the roofscape of the historic core are significant as the context and foil to these landmarks".

5.22 Whilst in the proposed scheme there is a further floor added, this is setback typically some 1.5 m from the building edge, and only extends above part of the proposed building. The key views analysis reasonably demonstrates that as one were to move around Clifford's Tower there would briefly, at a certain point around the south-east corner, be a temporary loss of view of the church tower. However as one were to perambulate around Clifford's Tower views of the upper section of the tower would remain. The hotel under construction at 46-50 Piccadilly will also have a considerable effect on this view and compete with the church. The introduction of taller buildings along Piccadilly is evidently affecting the view from Clifford's Tower. As a direct consequence of this building, the impact on the view of St Denys's and therefore its setting, would be very minor. However a minor impact does equate to 'less than substantial harm'.

Character and appearance of the conservation area

5.23 The scheme will have effect on two of the character areas within the Central Historic Core Conservation Area the Fossgate and Walmgate area and Piccadilly area. Aside from the impact on the views from Clifford's Tower, as assessed in the previous paragraphs, the scheme would preserve the character and appearance of both areas.

5.24 Fossgate and Walmgate (to the extent they are within character area 15) are lined predominantly with domestic scaled buildings dating from the 18th and 19th century. St Denys's Church sits within this townscape, and is identified as a landmark building.

5.25 To respect this townscape the proposed building steps down to 4-storey, lower than the existing building, and the buildings detailing breaks up the scale of the elevation which would form a backdrop to the church. Due to the scale, detail and materials proposed the building would maintain the character and appearance of the conservation area.

5.26 Piccadilly is a street with 20th century origins. Building plots are larger than those found elsewhere in the Central Historic Core Conservation Area. The character appraisal states “at the southern end, the street’s character is dominated by large modern office blocks which obscure the view to the Castle beyond. A number of buildings are in poor condition suggesting neglect and lack of investment over many years”. There are no listed buildings in the character area and what was the only identified building of merit – the former trolleybus depot has since been demolished. The area is not identified as having any “strengths” in the area appraisal.

5.27 The proposed building would sit alongside taller buildings; Ryedale House and Piccadilly Residence (the latter on the opposite side of St Denys Road) that have been recently refurbished and other buildings upwards of 5-storey of late 20th century and more recent vernacular. The proposed building would be 6-storey where fronting Piccadilly, clad in brick and of contemporary appearance. The development would introduce an active frontage onto Piccadilly and the scheme includes public realm enhancement. The scale, materials and detailing of the building would sit comfortably in the emerging context. No harm is identified to the Piccadilly character area.

Archaeology

5.28 The site is within the City Centre Area of Archaeological Importance. NPPF paragraph 189 states that “where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation”.

5.29 There have been multiple site investigations to inform this scheme; a borehole survey, ground penetrating radar (GPR) survey and evaluation trenches within the basement. As a consequence of this investigation conditions are proposed, to comply with the local requirement to preserve 95% of archaeology, and for a watching brief on groundworks.

Consideration of Public Benefits

5.30 Because the scheme has been deemed to have ‘less than substantial harm’ in terms of its impact on designated heritage assets this has to be weighed against the public benefits in order to conclude whether the scheme can be deemed NPPF compliant.

5.31 The identified harm is as a consequence of the proposed top floor of the building, which will mean that from a certain point, the view of the top section of the church tower from Clifford’s Tower will be obscured. As the effect is temporary in what is a panoramic view from Clifford’s Tower, and given the visible extent of the

Church affected, the level of harm is considered to be very low, even when paying special regard to the importance of the setting, as required by the Act.

5.32 The development would deliver the economic objectives of the NPPF in terms of making more efficient use of the site and enabling its re-use, for a commercial sector which is experiencing growth. This re-use of the site fits with the objectives of the city's economic strategy as reported in the 2018 draft local plan. There also wider environmental and social benefits in that the scheme includes public realm improvements, in accordance with the aspirations for Castle Piccadilly, again as set out in the 2018 draft local plan.

5.33 Given the low level of harm to heritage assets the public benefits identified do outweigh the identified harm.

IMPACT ON RESIDENTIAL AMENITY

5.34 The NPPF states that developments should create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users; and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience.

5.35 The site is within the defined city centre, where hotel uses are appropriate in principle, as defined in both national and draft local policy. With regards town centres the NPPF policy is based on ensuring vitality and that the need for 'town centre uses' can be accommodated within the city centre. This policy context has to be borne in mind when assessing the impact on surrounding residential uses.

5.36 The proposed building is essentially, in volume/height, part 1-storey lower than the existing and part 1-storey higher. Where it is a storey higher, this accommodation is set back from the main building edge and furthest from surrounding houses. In relation to the existing building, the proposed building will not be materially any more over-bearing or over-dominant and nor would it have an impact on daylighting of surrounding buildings.

5.37 The building is no closer to neighbouring houses than the existing. It is approximately 19 m from the front elevation of houses on Nelson's Yard and 30 m from Piccadilly residence. 59 St Denys Road is the closest neighbour, being around 13 m away; the buildings would not directly face each other. The level of overlooking that will occur between buildings is akin to that would be expected in a densely populated part of the city centre such as this.

5.38 In terms of noise, as a consequence of the hotel operation, the building will front onto Piccadilly which is a commercial street with multiple hotels of a similar scale. The level of associated activity, including servicing has to be expected in this

part of the city centre. In mitigation a condition is proposed to prevent waste (such as glass) being emptied at night-time and the plans show a drop off bay by the main entrance on Piccadilly, which will most likely be used for taxis.

HIGHWAY NETWORK MANAGEMENT

5.39 The NPPF states that in assessing applications for development, it should be ensured that:

- Appropriate opportunities to promote sustainable transport included.
- Safe and suitable access to the site can be achieved for all users.
- Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree.

5.40 The NFFPF also states “Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

Sustainable travel

5.41 It is a Council aspiration to reduce private car use in the city centre and it is reasonable to expect that guests and staff associated with a hotel in a city centre location such as this, use alternative modes of travel. There is also adequate car parking nearby within walking distance. There is a current planning application for a car park with 372 spaces at St Georges Field. The scheme would be able to accommodate bus drop off on St Denys and taxi / vehicle drop-off on Piccadilly. Staff cycle parking will provided within the building. 14 spaces are shown on plan with a further 6 spaces outside on Piccadilly. A condition will require a travel plan, which will promote sustainable travel to staff and explain how it will be communicated to guests that there is no parking on site and detail available alternatives.

Access

5.42 The building would have inclusive access.

Impacts on the network

5.43 Servicing is proposed from St Denys road. Servicing could occur under existing arrangements (where vehicles can stop for 10 minutes) but will be formally accommodated through a Traffic Regulation Order. The road has two lanes, both one way towards Piccadilly, and there is space to accommodate servicing vehicles.

The area where servicing is proposed is double yellow currently so there would be no loss of existing parking spaces.

5.44 A planning condition will require that the Travel Plan for the site explains how guests are aware that there is no car parking on site and how to access the site alternatively or make use of local car parks. This is not uncommon at city centre hotels; the Indigo hotel at 88-96 Walmgate has some 100 bedrooms and operates in a similar way. In terms of volume of traffic on the network, the impacts would potentially be similar for varying alternative uses for the site; the extant office use, or residential use, which could occur under permitted development. For example, the TRICS database (which can be used to estimate traffic use) if applied to this site would predict 30 vehicle trips at AM peak for the existing office, compared to 27 vehicle trips for the proposed hotel.

DRAINAGE / FLOOD RISK

5.45 The approach to managing flood risk set out at Section 14 of the NPPF is to avoid inappropriate development in areas at risk of flooding by directing development away from areas at highest risk, but where development is necessary, making it safe without increasing flood risk elsewhere. The proposed building is within Flood Zone 2. In national policy terms the proposed use is 'more vulnerable' which is appropriate in principle in Flood Zone 2. However the sequential test has to be passed and a site specific FRA is required to ensure the development will be safe from flood risk and would not increase flood risk elsewhere.

5.46 The scheme would be compliant with the policy for flood risk in the NPPF. The site is already developed and the scheme has a beneficial impact in that it will be reasonably safe from flooding for its lifetime and surface water run-off from site will be reduced, therefore reducing the risk of flooding elsewhere.

Sequential Test

5.47 According to the NPPF, the aim of the sequential test is to steer new development to areas with the lowest risk of flooding; development should not be permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower risk of flooding. The sequential test is considered to be passed. This is because the site is already developed and given the Local Plan aspirations for regeneration of the Castle Gateway area as part of a vibrant city centre.

Protection from flooding

5.48 The 1 in 100 year flood level 10.05 m AOD. Local recommendations is for FFL to be set 600 mm above the 1 in 100 level. The ground floor level, where bedrooms are proposed is at 11 AOD and consequently reasonably safe from flood risk.

Planning conditions can be applied in terms of avoiding future risk and flood resilient construction to the building. Access and egress routes are shown onto Dennis Street, above the 1 in 100 flood event as recommended by national planning guidance.

Flood risk elsewhere

5.49 The strategy proposes surface water run-off from the site to be reduced by 30% as required by local policy (this also achieves BREEAM points).

PUBLIC PROTECTION

Noise

5.50 Noise levels within bedrooms meet the British Standards expected for dwellings. This would accord with the NPPF (para 180) which states planning decisions should “mitigate and reduce to a minimum potential adverse impacts resulting from noise from new development – and avoid noise giving rise to significant adverse impacts on health and the quality of life”.

Land contamination

5.51 Conditions will require a full site investigation and implementation of an appropriate remediation strategy. The requirement accords with NPPF paragraph 178 which states that planning should ensure a site is suitable for its proposed use taking account of ground conditions and any risks arising from land instability and contamination.

SUSTAINABLE DESIGN AND CONSTRUCTION

5.52 Local policies CC1 and CC2 are applicable to this development. These require the following –

Carbon emissions enhanced by 28% in relation to Building Regulations
BREEAM Excellent

5.53 The scheme includes documents on sustainable design that show intent to comply with local requirements. These requirements will be secured through planning conditions.

5.54 The building would have low/zero carbon technology in the form of mechanical ventilation/heat recovery, air-sourced heat pumps and PV panels. Combined with fabric efficiency the 28% carbon emission target can be met.

5.55 The BREEAM rating covers a range of topics - management, health and well-being, energy, transport, water efficiency, materials, waste, land use and ecology, pollution and innovation. The pre-assessment report shows the intention to achieve BREEAM Excellent.

ECOLOGY

5.56 Bat surveys were undertaken in 2018. The Ecologist reported that building has few features to support roosts and there was no evidence of roosting bats, and no bats were observed emerging from the building. However there is potential that bats could occupy the building and it does have features that could accommodate bats. A condition is proposed which sets out procedures for demolition and requires mitigation – bat boxes on the proposed building.

6.0 CONCLUSION

6.1 The proposed hotel use is acceptable in principle at this city centre site and fits with the aspirations for economic growth in the NPPF and the 2018 DLP, by facilitating a sector where there is growth and evidentially demand. The scheme is appropriate for the site; the design is acceptable and relates to the context.

6.2 There would be a very low level of harm to designated Heritage Assets, which is considered to be outweighed by the public benefits of the scheme. The benefits are predominantly economic but are also environmental and social given the public realm enhancements involved.

6.2 With regards demolition, given the acceptable impact on Heritage Assets, there are no policy grounds to oppose this; in accordance with NPPF advice the re-development makes more efficient use of the site (providing additional floor-space) and the new build will comply with Local Plan policies on Sustainable Design and Construction. A condition will prevent any premature demolition, before there is a contract in place for the construction project.

6.3 There would be no unacceptable impact on amenity, which cannot be reasonably controlled through the use of planning conditions. Other technical matters can also be dealt with, to the extent the scheme would be NPPF compliant by way of conditions.

7.0 RECOMMENDATION: Approve

1 TIME2 Development start within three years

2 The development hereby permitted shall be carried out in accordance with the following plans:-

Site Plan - A-100-100 P0

Floor Plans - A-100- 099 P2, 100 P2, 101 P2, 103 P1, 104 P2, 105 P2, 106 P1

Elevations - A-100 - 001 P1, 002 P1, 003 P1, 004 P1, 010 P1, 011 P1

Sections - A-120 - 001 P2, 002 P1, 003 P1, 010 P1

Reason: For the avoidance of doubt and to ensure that the development is carried out only as approved by the Local Planning Authority.

3 Demolition only when contract in place for redevelopment

Prior to any demolition works a binding contract for the carrying out and completion of works of redevelopment of the site, for which planning permission has been granted, shall be entered into, and evidence of that contract submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure the demolition is followed by immediate rebuilding; to maintain the character and appearance of the Conservation Area and to prevent the unnecessary loss of office space.

4 Construction management

Prior to commencement of development a Construction Management Plan shall be submitted to and approved in writing by the local planning authority. The approved plan shall be strictly adhered to throughout the construction period of the development. The plan shall include the following details-

Dilapidation survey

A dilapidation survey of the highways adjoining the site which shall be jointly undertaken with the Council's highways department and the results of which shall be agreed in writing with the Local Planning Authority.

Management of vehicular movements associated with construction and contractor parking.

Wheel washing facilities to prevent mud and detritus getting on to the public highway.

Measures to control the emission of noise, dust and dirt during construction.

To include a site specific risk assessment of dust impacts in line with the guidance provided by IAQM (see <http://iaqm.co.uk/guidance/>) and a package of mitigation measures commensurate with the risk identified in the assessment and measures to control noise during any piling of foundations.

A scheme for recycling/disposing of waste resulting from construction works.

Means of preventing light pollution, including the angling of lighting and times of operation.

Point of contact on site for enquiries.

A complaints procedure.

The procedure should detail how a contact number will be advertised to the public, and procedure once a complaint had been received. Written records of any complaints received and actions taken should be kept and details forwarded to the Local Authority every month during construction works by email to the following addresses public.protection@york.gov.uk and planning.enforcement@york.gov.uk

Reason: To protect the amenity of the locality.

5 Times of construction

The hours of construction, loading or unloading on the site shall be confined to 8:00 to 18:00 Monday to Friday, 9:00 to 13:00 Saturday and no working on Sundays or public holidays.

Any working outside of the permitted hours is subject to prior approval in writing by the Local Planning Authority. (It is asked that any requests to work outside of the permitted hours contains justification and details of practical measures to avoid noise disturbance).

Reason: To protect the amenities of adjacent residents.

6 Archaeology - watching brief

A programme of post-determination archaeological mitigation, specifically an archaeological watching brief and excavation (the latter if required) is required.

A) No groundworks (including grubbing up of foundations or ground breaking works) shall take place until a written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. For land that is included within the WSI, No grubbing up of foundations or ground breaking works shall take place other than in accordance with the agreed WSI. The WSI should conform to standards set by LPA and the Chartered Institute for Archaeologists.

B) The site investigation and post investigation assessment shall be completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (A) and the provision made for analysis, publication and dissemination of results and archive deposition will be secured. This part of the

condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI.

C) A copy of a report (and evidence for publication if required) shall be deposited with City of York Historic Environment Record to allow public dissemination of results within 3 months of completion or such other period as may be agreed in writing with the Local Planning Authority.

Reason: This condition is imposed in accordance with Section 16 of NPPF. The site lies within an Area of Archaeological Importance and the development may affect important archaeological deposits which must be recorded prior to destruction.

7 Archaeology

A foundation design and statement of working methods, which preserve 95% of the archaeological deposits, is required for this site.

No groundworks shall commence until a foundation design and statement of working methods (including a methodology for identifying and dealing with obstructions to piles and specification of a level in metres AOD below which no destruction or disturbance shall be made to archaeological deposits except for that caused by the boring or auguring of piles for the building foundation) which preserve 95% of the archaeological deposits on the site has been submitted to and approved in writing by the Local Planning Authority. All works shall be carried out in accordance with the approved foundation design and statement of working methods.

Reason: This condition is imposed in accordance with Section 16 of NPPF and City of York Historic Environment Policy HE10 or D6 of the publication draft Local Plan. The site lies within an Area of Archaeological Importance or the site is of Archaeological Interest which contains significant archaeological deposits. The development must be designed to preserve 95% of the archaeological deposits within the footprint of the building(s).

8 LC1 Land contamination - Site investigation

9 LC2 Land contamination - remediation scheme

10 LC3 Land contamination - remedial works

11 Large scale details

Large scale drawings, including sections showing typical details of the building facade shall be submitted to and approved in writing by the Local Planning Authority prior to commencement of construction and the works shall be carried out in accordance with the approved details. Details shall include any permanently fixed equipment for

servicing and maintenance (ladders, guarding etc). Note these will generally not be expected unless already on the permitted drawings or are not visibly intrusive.

Reason: In the interests of good design, in accordance with NPPF paragraph 127.

12 Materials

Samples of the external materials to be used shall approved in writing by the Local Planning Authority prior to the commencement of construction. The development shall be carried out using the approved materials. Details shall include manufacturer's details and colour finish. Samples shall be provided on site for approval as required by the local planning authority.

A sample panel of the brickwork to be used on the building shall be erected on the site and shall illustrate the colour, texture and bonding of brickwork and the mortar treatment to be used, and shall be approved in writing by the Local Planning Authority prior to the commencement of construction. The panel(s) shall be retained until a minimum of 2 square metres of wall of the approved development has been completed in accordance with the approved sample.

Reason: In the interests of good design and visual amenity, in accordance with the NPPF, paragraph 127.

13 Sustainable construction

The development hereby permitted shall achieve a reduction in carbon emissions of at least 28% compared to the target emission rate as required under Part L of the Building Regulations.

Prior to first use details of the measures undertaken to secure compliance with this condition shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details.

Reason: To fulfil the environmental objectives of the NPPF and support the transition to a low carbon future, and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

14 BREEAM

The development hereby permitted shall achieve a BREEAM rating of at least excellent.

A Post Construction Assessment by a licensed BREEAM assessor shall be carried out and a copy of the certificate submitted to the Local Planning Authority within 12 months of first use (unless otherwise agreed). Should the development fail to achieve

an 'Excellent' BREEAM rating a report shall be submitted for the written approval of the Local Planning Authority demonstrating what remedial measures shall be undertaken to achieve an 'Excellent' rating. The remedial measures shall then be undertaken within a timescale to be approved in writing by the Local Planning Authority.'

Reason: To fulfil the environmental objectives of the NPPF and support the transition to a low carbon future, and in accordance with policies CC1 and CC2 of the Publication Draft Local Plan 2018.

15 Cycle parking

Full details of the cycle parking facilities within the building shall be submitted to and approved in writing by the Local Planning Authority and installed in accordance with the approved details prior to first use of the development hereby permitted. The cycle parking facilities shall be retained thereafter. Details shall include the following -

- Access door, which shall be at least 1 m wide. Details of secure opening and closing / access control to be provided.
- Measures to facilitate level (ramped) access where practical otherwise measures to assist cycles travel the steps.
- The provision and type of securing cycles shall be at least as per the approved floor plans.
- Facilities to allow electric charging.

Reason: In order to deliver sustainable transport objectives of the NPPF and the draft local plan.

16 Public realm improvements / external works

Details of the highway works and public realm improvements (the widening of footpaths, provision of drop-off, landscaping and street furniture) as indicatively shown on the approved ground floor plan and a timescale for implementation shall be submitted to and approved in writing by the Local Planning Authority prior to the first occupation. The works shall be carried out in accordance with the approved details and timescale thereafter.

Reason: In the interests of promoting sustainable modes of travel, highway safety and to secure regeneration improvements to Piccadilly proportionate to the development proposed in accordance with draft local plan policy SS5.

Notes

- The developer will also require approval from the Highway Authority for the works being proposed, under the Highways Act 1980.
- Works are expected to widen the footpath in accordance with WSP plan 70034291-

PIC-GA-1 P01, which includes the narrowing of St Denys Rd to 6 m at the junction with Piccadilly (or any updated version of this plan).

17 The development hereby permitted shall not come into use until the following highway works (which definition shall include works associated with any Traffic Regulation Order required as a result of the development, signing, lighting, drainage and other related works) have been carried out in accordance with the approved plans, or arrangements entered into which ensure the same.

Highway works - Provision of servicing / loading bay on St Denys Road

Reason: In the interests of good design and the safe and free passage of highway users.

18 Noise insulation

The building shall be constructed so as to achieve internal noise levels in guest rooms of no greater than 35 dB LAeq (16 hour) during the day (07:00-23:00 hrs) and at night (23:00 - 07:00 hours) 30 dB LAeq (8 hour). These noise levels shall be observed with all windows open in the relevant rooms or if necessary windows closed and other means of ventilation provided.

Reason: In the interests of good design and the amenity of future users of the building.

19 Cooking Odours

There shall be adequate facilities for the treatment and extraction of cooking odours. Details of the extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for written approval. Once approved it shall be installed and fully operational before the proposed use first opens and shall be appropriately maintained and serviced thereafter in accordance with manufacturer guidelines.

Reason: To protect the amenity of nearby properties and the environmental qualities of the area.

Note: It is recommended that the applicant refers to the updated Guidance produced by EMAQ in September 2018 titled "Control of Odour and Noise from Commercial Kitchen Exhaust Systems (September 2018)" for further advice on how to comply with this condition. The applicant shall provide information on the location and level of the proposed extraction discharge, the proximity of receptors, size of kitchen or number of covers, and the types of food proposed. A risk assessment in accordance with APPENDIX 3 of the EMAQ guidance shall then be undertaken to determine the level of odour control required. Details should then be provided on the location and size/capacity of any proposed methods of odour control, such as filters, electrostatic

precipitation, carbon filters, ultraviolet light/ozone treatment, or odour neutraliser, and include details on the predicted air flow rates in m³/s throughout the extraction system.

20 Travel plan

The development hereby permitted shall not be brought into use until a Travel Plan, specific to the site operator, has been submitted to and approved in writing by the Local Planning Authority.

The travel plan shall explain how guests will be advised that the site has no car parking on-site and how they can alternatively access the site. This shall encourage and promote sustainable modes of travel and be reasonably updated over the lifetime of the development.

It shall also contain details as to how staff will be encouraged and incentivised to use sustainable modes of travel and include targets and monitoring in accordance with national planning guidance on travel plans.

The approved Travel Plan shall be implemented in all respects following the commencement of the use hereby approved.

Reason: In order to deliver sustainable transport objectives of the NPPF and the draft local plan.

21 LC4 Land contamination - unexpected contamination

22 Surface water drainage

Site drainage shall be provided in accordance with the drainage strategy in the Dudleys FRA01 revision B, unless an alternative scheme is approved in writing by the Local Planning Authority. The strategy shows run off restricted to 8.7 l/s and 17 sq m of attenuation provided on site.

Reason: In order to avoid increased flood risk elsewhere in accordance with Publication Draft Local Plan policy ENV4 and NPPF paragraph 163.

23 Flood Resilience

The development shall include the flood resilient measures (as recommended in the Dudleys FRA01 revision B) as follows -

- There shall be no sleeping accommodation below 11 m AOD
- Access and egress / evacuation routes shall be provided which are set at a level in excess of the 1 in 100 year flood level.

- Flood resilience to the basement, specifically regarding equipment required to allow operation of the building, shall be considered and the development carried out accordingly.

Reason: To protect against flood risk in accordance with NPPF paragraph 163.

24 Ecology / Bats

Demolition shall occur in accordance with the method statement in section 7 of the Wold Ecology Bat Activity Survey Report 2018 (or alternative strategy with prior approval from the Local Planning Authority).

At least 2 bat boxes shall be incorporated on the building hereby permitted, which shall be installed prior to first use. The bat boxes shall be Schwegler bat boxes or similar and shall be installed south, east or west orientated elevations and at least 3 m from ground level.

Reason: In accordance with NPPF paragraph 170 to minimise impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures.

25 Noise - waste

There shall be no putting out of rubbish / emptying of bins within any external area between the hours of 23:00 and 07:00 the following day.

Reason: To avoid noise disturbance, such as the smashing of glass, that would cause undue disturbance to surrounding occupants.

26 HWAY29 No door or gate etc to open in highway

8.0 INFORMATIVES:

Notes to Applicant

1. STATEMENT OF THE COUNCIL`S POSITIVE AND PROACTIVE APPROACH

In considering the application, the Local Planning Authority has implemented the requirements set out within the National Planning Policy Framework (paragraph 38) in seeking solutions to problems identified during the processing of the application. The Local Planning Authority took the following steps in order to achieve a positive outcome: sought revised plans to address objections and through the use of planning conditions.

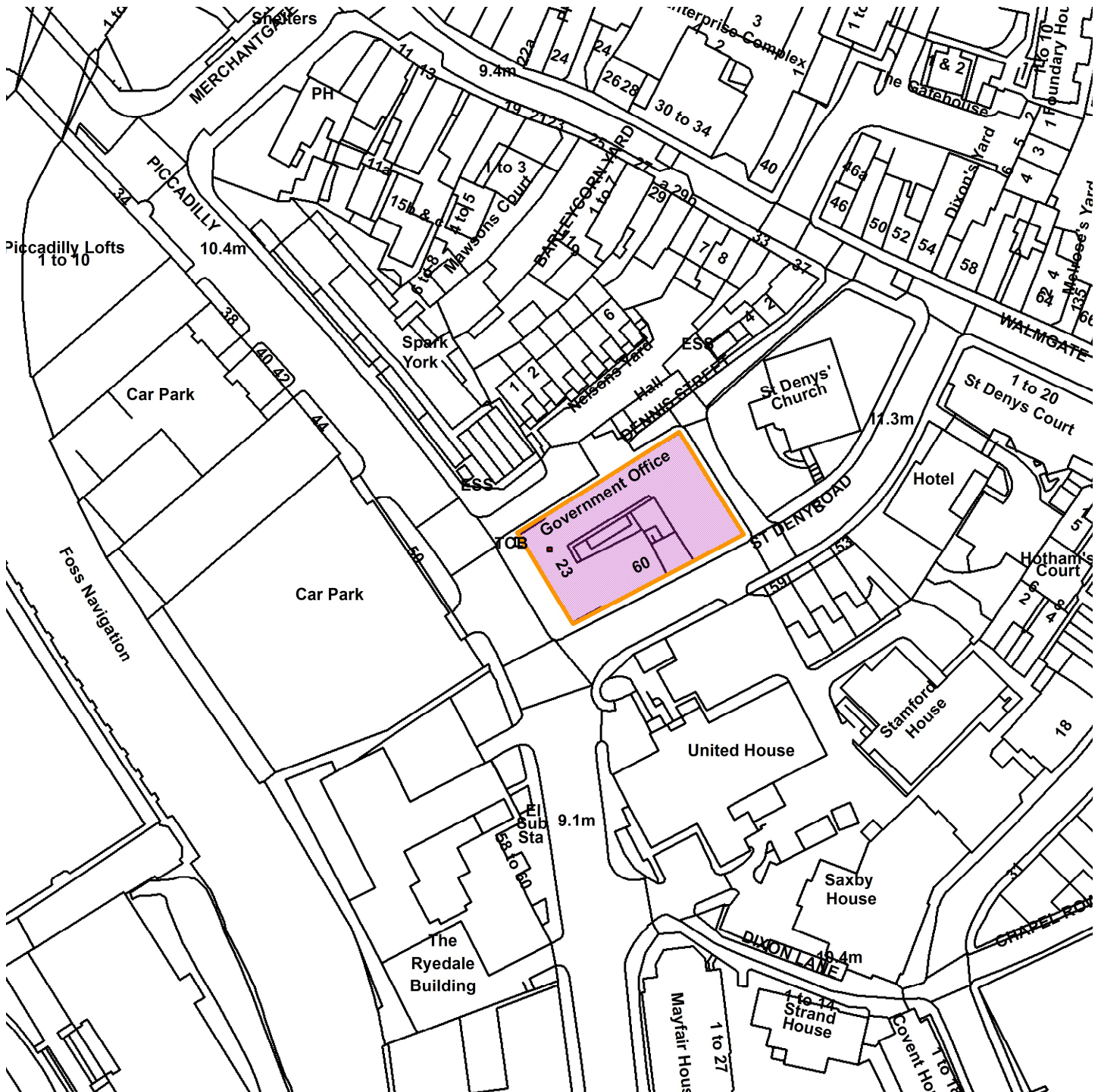
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23 Piccadilly, YO1 9PG

19/02563/FULM



GIS by ESRI (UK)



Scale : 1:1153

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Organisation	City of York Council
Department	Economy & Place
Comments	Site Location Plant
Date	24 July 2020
SLA Number	

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Planning Committee - Thursday 13 August 2020

19/02563/FULM

23 Piccadilly

View North
along Piccadilly



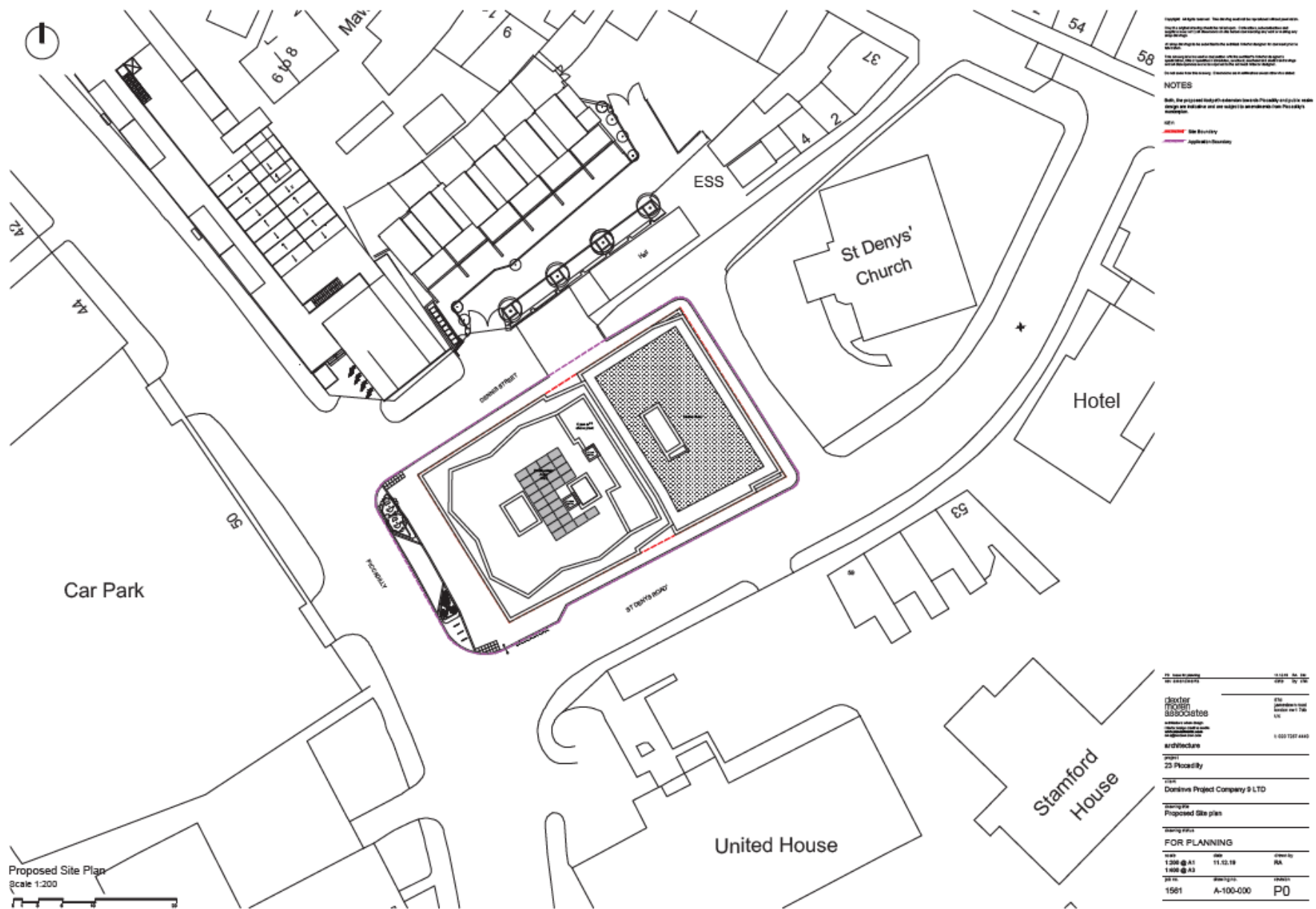
City of York Council Planning Committee - Thursday 13 August
2020

View South
Along Piccadilly



View from Walmgate

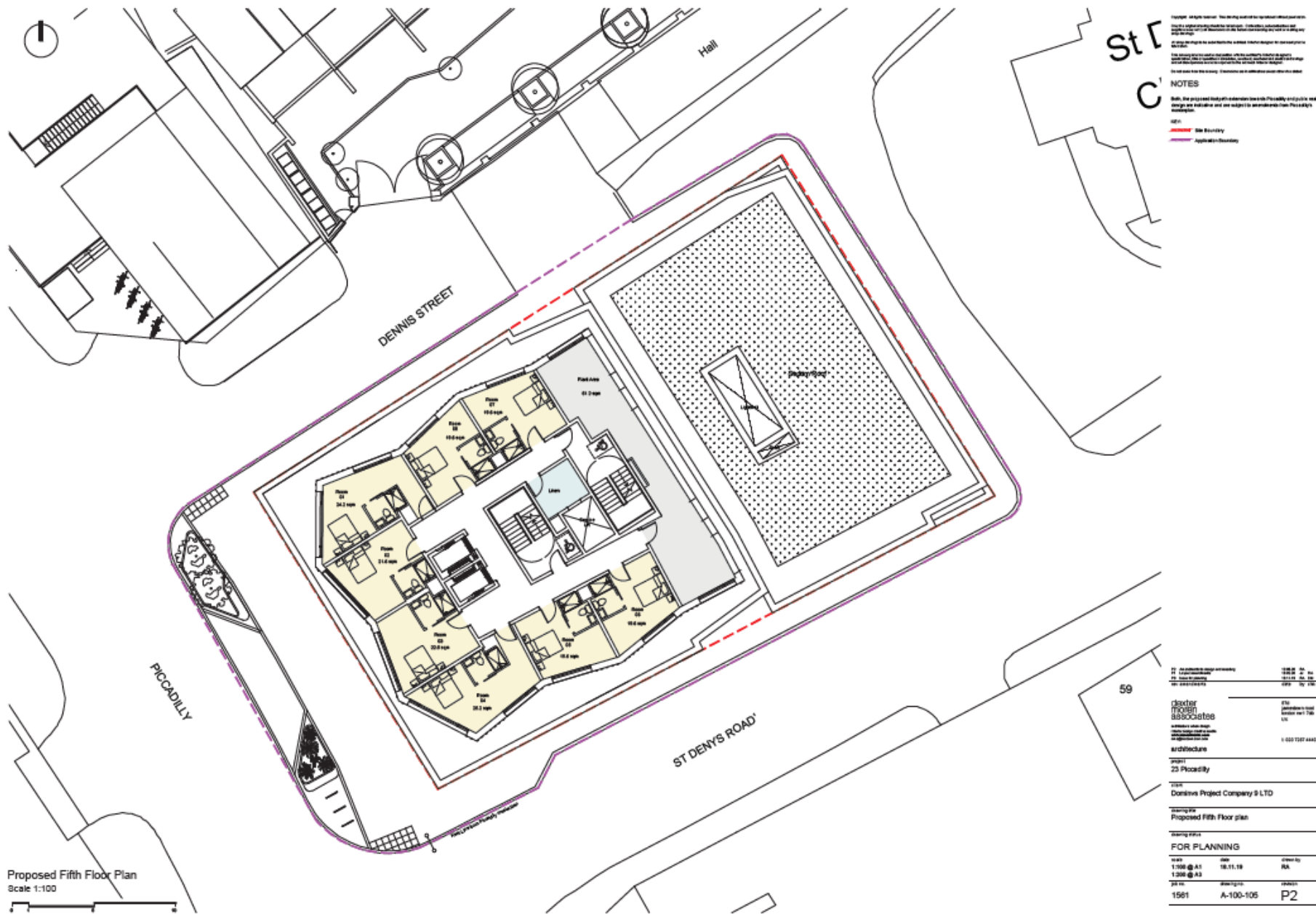




Ground Floor Plan



Fifth Floor Plan



Piccadilly Elevation



Project: 19-1110-004
 Drawing No: A-110-004
 Date: 10/11/19
 Scale: 1:100
 Project Name: 23 Piccadilly
 Client: Dexus Project Company (UK) Ltd
 Architect: dexter robyn associates

NOTES



MATERIAL KEY

- 1 - Red brickwork (existing)
- 2 - Red brickwork (proposed)
- 3 - Red brickwork (proposed)
- 4 - Recessed brickwork (proposed)
- 5 - Anodised Aluminium (dark grey) (proposed)
- 6 - Unisex claret grey

Client	Dexus Project Company (UK) Ltd
Project Name	23 Piccadilly
Project Address	23 Piccadilly, York, YO1 1LW
Project No.	A-110-004
Project Date	10/11/19
Project Scale	1:100
Project Status	FOR PLANNING
Project No.	1961
Project Name	A-110-004
Project Date	P1

St Denys Road Elevation



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For further information, please contact the architect.



- NOTES**
- MATERIAL KEY**
- 1 - Flat infillwork shading
 - 2 - Stippled masonry brickwork
 - 3 - Rusticated brickwork
 - 4 - Rusticated brickwork horizontal detail
 - 5 - Architectural Masonry: dark grey rusticated window frame
 - 6 - Concrete (dark grey)

Proposed St Denys' Road Elevation
Scale 1:100

Architect	dexter moun ASSOCIATES	Architect No.	10012017-0001
Project	23 Piccadilly	Client	Denarius Project Company (UK) LTD
Project No.	10012017-0001	Project Name	Proposed St Denys' Road Elevation
Scale	1:100 @ A1 1:200 @ A3	Date	10.11.19
Job No.	1951	Revision	P1

Dennis Street Elevation



Proposed Dennis Street Elevation
Scale 1:100

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NOTES

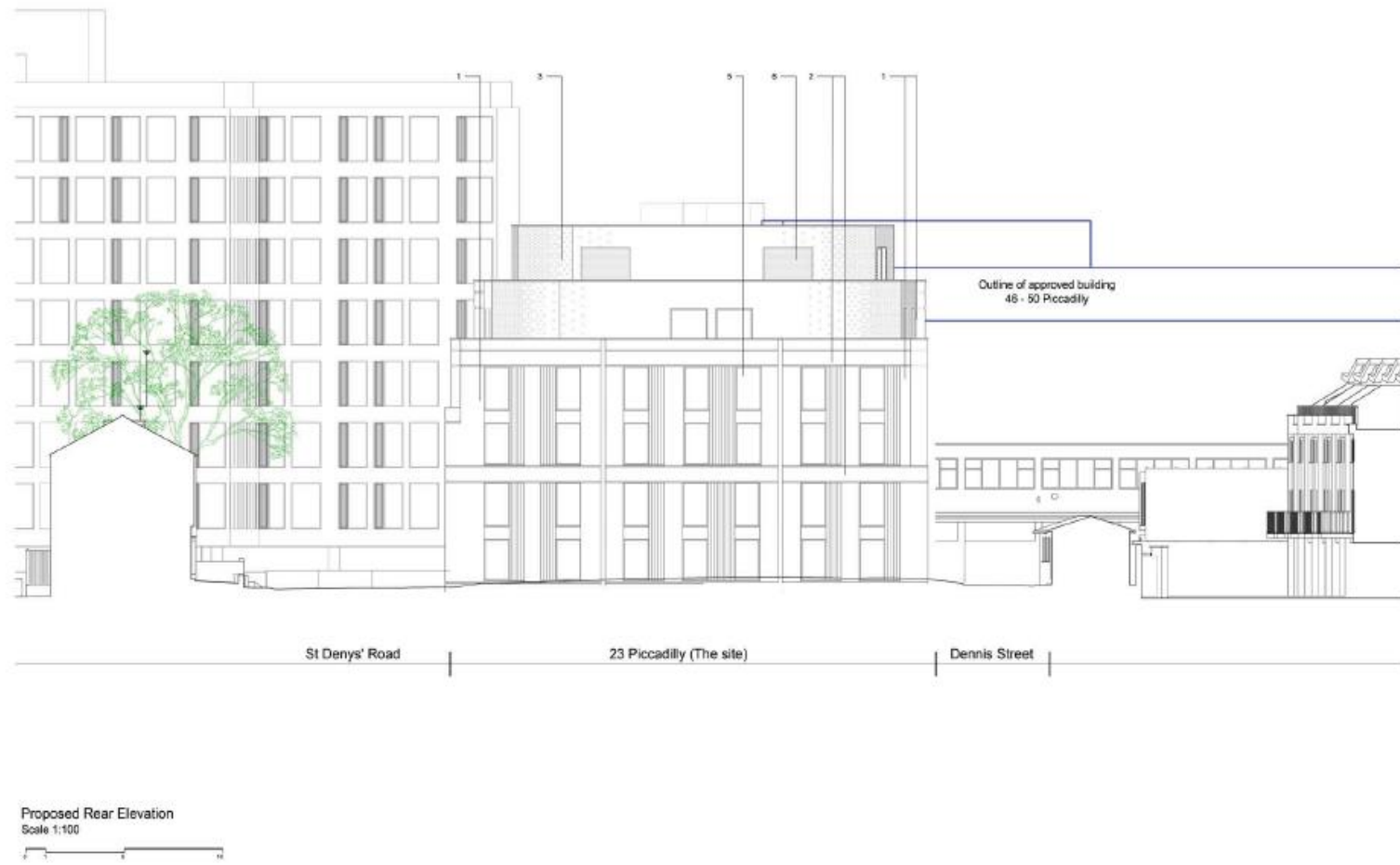


HISTORICAL KEY

- 1. First window detailing
- 2. Second window detailing
- 3. Facaded windows
- 4. Facaded windows (horizontal detail)
- 5. Facaded windows (dark grey) (vertical window frame)
- 6. Louvre dark grey

10. Architectural drawings	0000	00
11. Date of drawing	07/18	00
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Rear Elevation



Notes: This drawing is for information only and does not constitute an offer of any services. It is subject to the terms and conditions of the contract. The design is based on the information provided and is not intended to be used for any other purpose. The design is based on the information provided and is not intended to be used for any other purpose.

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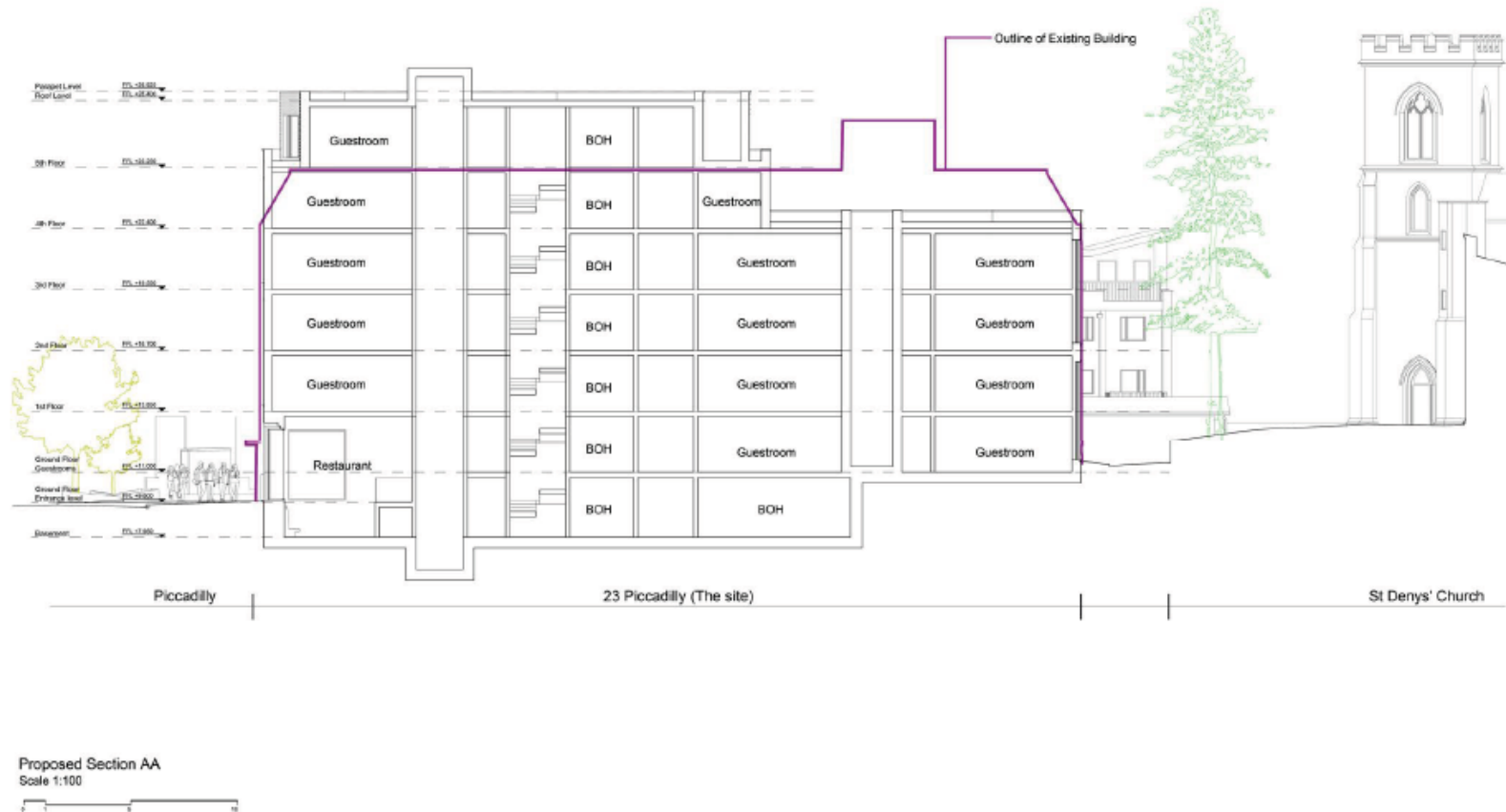


MATERIAL KEY

- 1 - Flat (existing) building
- 2 - Building facade (existing)
- 3 - Flat (existing) building
- 4 - Flat (existing) building
- 5 - Flat (existing) building
- 6 - Flat (existing) building

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Project Name	23 Piccadilly	
Client	Deimus Project Company Ltd	
Planning Ref	Proposed Rear Elevation	
Planning Status	FOR PLANNING	
Date	11.11.18	
Scale	1:100 @ A1, 1:200 @ A3	
Project No	1061	
Revision No	A-110-003	
Revision	P1	

Cross-section with outline of Existing Building



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Client	Derinix Project Company © LTD
Project Name	Proposed Section AA
Project Number	FOR PLANNING
Scale	1:100 @ A1 1:300 @ A3
Date	10.11.19
Drawn by	RA
Checked by	
Job No.	A-120-001
Revision	P2



CORNER VIEW OF PICCADILLY WITH DENNIS ST



CORNER VIEW OF PICCADILLY WITH ST DENYS ROAD